

**TOWN OF NEEDHAM
MASSACHUSETTS**



500 Dedham Avenue
Needham, MA 02492
781-455-7550

PLANNING BOARD

APPLICATION FOR SITE PLAN REVIEW

Project Determination: (circle one) Major Project Minor Project

This application must be completed, signed, and submitted with the filing fee by the applicant or his representative in accordance with the Planning Board's Rules as adopted under its jurisdiction as a Special Permit Granting Authority. Section 7.4 of the By-Laws.

Location of Property 1063 Great Plain Avenue/0 Garden Street
Name of Applicant Needham Bank
Applicant's Address 1063 Great Plain Avenue, Needham, MA 02492
Phone Number 781-474-5848

Applicant is: Owner X Tenant _____
Agent/Attorney _____ Purchaser _____

Property Owner's Name Needham Bank
Property Owner's Address 1063 Great Plain Avenue, Needham, MA 02492
Telephone Number 781-474-5898

Characteristics of Property: Lot Area 44978 Present Use Bank with drive-through ATM
Map # 51 Parcel # 86 Zoning District CB/CSB

Description of Project for Site Plan Review under Section 7.4 of the Zoning By-Law:

Conversion of existing mezzanine in Bank building into 1,365 square feet of floor area for executive offices and demolition of existing drive-through free-standing automatic teller machine (ATM) and construction of a 321 square foot drive-up teller building with an ATM.

Signature of Applicant (or representative) _____

Address if not applicant _____

Telephone # _____

Owner's permission if other than applicant _____

[Handwritten Signature]
VP - Facilities Director
Needham Bank

SUMMARY OF PLANNING BOARD ACTION

Received by Planning Board _____ Date _____
Hearing Date _____ Parties of Interest Notified of Public Hearing _____
Decision Required by _____ Decision/Notices of Decision sent _____
Granted _____
Denied _____ Fee Paid _____ Fee Waived _____
Withdrawn _____

NOTE: Reports on Minor Projects must be issued within 35 days of filing date.

ADDENDUM TO
APPLICATION FOR SITE PLAN REVIEW

APPLICANT: NEEDHAM BANK

PROPERTY: 1063 GREAT PLAIN AVENUE AND 0 GARDEN STREET

Needham Bank (the “Bank”) respectfully submits this Addendum in support of and in furtherance of its Application for Site Plan Review and for the waivers and special permits enumerated herein.

I. INTRODUCTION AND OVERVIEW OF PROJECT

The Bank is the owner of the real estate known and numbered as numbered as 1063 Great Plain Avenue (the “Subject Property”) and shown on Needham Assessors Map 51, Lot 86¹. The Subject Property contains approximately 44,978 square feet of land with frontage on Great Plain Avenue and Garden Street and is currently occupied by an approximate 34,580 square foot bank building as well as a free-standing drive-up automatic teller machine (ATM). The Subject Property is currently served by 62 parking spaces (described further below). The Subject Property is located in the Center Business (CB) Zoning District and the Chestnut Street Business (CSB) Zoning District.

As described in further detail in this Addendum, this Project involves an increase of approximately 1,686 square feet of floor area at the “Subject Property” consisting of the following: (a) an existing mezzanine area in the Bank building will be converted to executive offices by adding approximately 1,365 square feet of floor area, and (b) the existing drive-up free-standing automatic teller machine (“ATM”) will be removed and an approximate 321 square foot drive-up teller building with a new drive up ATM will be constructed. Upon completion of the Project, the Subject Property will be served by 57 parking spaces.

The Bank has been an institution in the Town of Needham since 1892. In 1922 it commenced occupancy of its current location at 1063 Great Plain Avenue. In 1997, it purchased a lot on Garden Street for use as an ancillary parking lot. This was approved by the Planning Board on May 19, 1997. (*See, Major Project Site Plan Review Special Permit No. 1997-03*) In November 1997, the Bank purchased the adjacent building at 1055 Great Plain Avenue. On July 20, 2006, the Planning Board authorized the Bank to renovate and connect the buildings at 1055 and 1063 Great Plain Avenue. (*See, Major*

¹ The Bank is also the owner of the real estate known and numbered as 0 Garden Street and shown on Needham Assessors Map 51, Lot 22. This property contains approximately 10,019 square feet of land and is currently utilized as a satellite parking lot for the Bank. With this Project, this property will continue to be so utilized. This property is sometimes referred to herein as the Garden Street Parking Lot.

Project Site Plan Review Special Permit No. 2006-02, as amended) The decision also authorized a new common entrance, an ATM enclosure, an increase in floor space, and various waivers and/or special permits including a reduction in the required number of parking spaces. In April 2007, the Bank purchased the adjacent building at 10 Eaton Square. On September 8, 2008, the Planning Board authorized the Bank to renovate and expand and connect this building to the buildings at 1055 and 1063 Great Plain Avenue. (See, *Major Project Site Plan Review Special Permit No. 2008-07*²) The decision also authorized various waivers and/or special permits for an increase in floor area ratio and reduction in the required number of parking spaces.

On October 22, 2010, the Bank purchased the property at 232 and 244 Garden Street containing of approximately 18,800 square feet (.4037 acres) of land upon and which was then occupied by two dwellings. At that time, this property was physically separated from the remaining Bank property by Eaton Square. However, on or about November 7, 2011, the Town of Needham abandoned and discontinued Eaton Square as a public way. With the abandonment and discontinuance of Eaton Square that portion of the roadway formerly separating the various Bank land is now in Bank ownership (and the entire property is now known as 1063 Great Plain Avenue). On June 12, 2012, the Planning Board authorized the Bank to construct an approximately 11,696 square foot addition to the Bank building and to construct a drive-up free-standing ATM. In connection with that project, the Bank expanded its parking facilities to encompass the former 232 and 244 Garden Street parcels and to redevelop the Town owned parking lot abutting the Bank property. (See, *Major Project Site Plan Review Special Permit No. 2012-04*). That project required 94 parking spaces. However, the decision approved 62 parking spaces (i.e., a waiver of 32 parking spaces) of which 35 parking spaces at located in the Bank lot at 1063 Great Plain Avenue and 27 parking spaces (including tandem spaces) are located at 0 Garden Street.

As a result of the COVID pandemic and technological improvements, there have been significant changes to the manner in which people conduct their banking activities. Specifically, Applicant has noticed a dramatic increase in the utilization of on-line and electronic banking with a corresponding decrease in the customers physically coming to the Bank itself. Furthermore, it is Applicant's experience that when customers do need to come to the Bank, it is a preference to utilize drive-through tellers and ATMs. The exception is when a customer needs to meet privately with Bank personnel (as opposed to simple deposits and withdrawals). In addition, during the COVID pandemic a majority of the Bank employees were required and able to work remotely. It is the Bank's intention to maintain this type of remote work for these employees at least a couple of days per week. To address the above, Applicant proposes to convert an existing mezzanine area in the Bank building to executive offices by adding approximately 1,365 square feet of floor area (thus freeing up other office space for use for private customer conferences). In addition, Applicant proposes to demolish the existing drive-up free-standing ATM, and replace the same with an approximate 321 square foot drive-up teller building with a

² On January 5, 2010, this Decision was amended to allow a design change for an archway over an entrance through an existing stone wall.

drive-up ATM. The new drive through building will be located in entirely in the CSB Zoning District.

As discussed further below, the Project requires 99 parking spaces. Upon completion of the Project, the Bank will be served by 57 parking spaces (a waiver of 42 parking spaces) of which 30 parking spaces will be located in the Bank lot at 1063 Great Plain Avenue and 27 parking spaces (including tandem spaces) are located at 0 Garden Street. (It is noteworthy that the Bank has and will continue to “lease” a substantial number of parking spaces in the Town owned lot.) For the reasons cited above, Applicant submits that 57 parking spaces are more than sufficient to accommodate the Applicant’s parking demand.

II. ZONING ANALYSIS

The subject property is located in the CB and CBS Zoning Districts. The Project is a construction project which creates or adds gross floor area, changes an exterior façade, and adds parking spaces. Pursuant to Section 7.4.2 of the Needham Zoning By-Law such projects in the CB or CBS Zoning District are considered a Major Project. Under Section 7.4.3 of the Needham Zoning By-Law, a Major Project requires a site plan review special permit from the Planning Board. Said Section 7.4.2 further provides that the Planning Board shall be the special permit granting authority for all permits the issuance of which is necessary for construction of use of a Major Project.

The Zoning By-Law for the Town of Needham prescribes the uses which are permitted as of right, the uses permitted only upon issuance of a special permit, and the uses not permitted in the CB and CSB Zoning Districts. Per the schedule of use regulations sets forth in Section 3.2.2 of the Needham Zoning By-Law, “banks” are allowed as of right in the CB and CSB Zoning Districts. The Needham Zoning By-Law is silent as to teller windows. However, the schedule of use regulations in said Section 3.2.2 provides that “other accessory uses incidental to lawful principle uses” are allowed in the CSB Zoning Districts by special permit. Section 1.3 (Definitions) of the Needham Zoning By-Law defines an “accessory building” as a “building devoted exclusively to a use subordinate to and customarily incidental to the principal use” and defines an “accessory use” as “a use subordinate to and customarily incidental to the principal use”. The proposed drive-up teller building with a drive-up ATM is an “accessory use” to the bank use and is therefore allowed by special permit in the CSB Zoning District.

Upon completion of the Project, the total floor area at the Subject will increase from the current 34,580 square feet to 36,266 square feet (i.e., an increase of 1,365 square feet). Currently, 24,007 square feet is dedicated to office use and 10,573 square feet is dedicated to utility/storage space. Upon completion of the Project there will be 25,693 square feet of office use and 10,573 square feet dedicated to utility/storage space. Per Section 5.1.2 of the Needham Zoning By-Law, banks require 1 parking space per 250 square feet of floor area and utility/storage requires 1 space per 850 square feet of floor area. Accordingly, the Bank currently requires 94 parking spaces. In accordance with the

2012 site plan approval, a waiver of 32 parking spaces was granted and the Bank is currently served by 62 parking spaces. Upon completion of this Project, the Bank will require 99 parking spaces and is providing 57 parking spaces. Therefore, a waiver of 42 parking spaces is required. As stated above, with the change in consumer banking practices as well as Applicant's own business practices (i.e., remote workers, etc.), Applicant submits that 57 parking spaces are more than sufficient. In addition, Applicant has and will continue to lease spaces in the abutting municipal lot.

The Needham Zoning By-Law prescribes dimensional requirements (lot area, frontage, setbacks, etc.) for the buildings and structures in the CB and CSB Zoning Districts. As shown on the submitted plans, the Project satisfies these dimensional requirements. All of the "changes" to the Bank building are internal to the same (and therefore there are no modifications to the same in terms of any required setbacks). The only "new" structure/building involved in the Project is the proposed drive-up teller building with the drive-up ATM and same has been setback at minimum of 10 feet from Garden Street.

As detailed further below, in addition to the site plan review and accessory use special permits, the Bank requires and requests the following relief:

- a. a special permit under Sections 5.1.1.5 and 5.1.1.6 of the Needham Zoning By-Law to waive strict adherence to the requirements of Section 5.1.2 and 5.1.3 of the Needham Zoning By-Law (required parking and parking plan and design requirements, respectively);
- b. a special permit under Section 3.2.2 of the Needham Zoning By-Law ("Schedule of Use Regulations") for "off-street parking for vehicles associated with a principle use, located on a separate lot owned or leased by the owner of the land in which the principle use is located, within a zoning district in which the principle use is permitted";
- c. A special permit to waive the landscape specifications set forth in Section 4.4.8.5 of the Needham Zoning By-Law
- d. a waiver under Section 7.4.4 of the Needham Zoning By-Law of the submission of certain supporting material and documentation with the application for Site Plan Review.

It is noteworthy the most (if not all) of the above have been previously granted to some extent to Applicant for previous developments at the Subject Property

III. NEED FOR VARIANCE

As required under Section 7.4.4 of the Needham Zoning By-Law, the Bank hereby certifies that the Project can be constructed and the proposed use thereof may commence without the need for the issuance of a variance from any provisions of the Needham Zoning By-Law by the Zoning Board of Appeals.

IV. REQUIRED SPECIAL PERMIT AND WAIVER RELIEF

As required by Section 7.4.4 of the Needham Zoning By-Law, the Bank submits the following itemized list of the sections of the Needham Zoning By-Law under which relief by special permit is or may be required for the construction or use of the Project:

A. PARKING: As indicated above, under the Needham Zoning By-Law the Project requires 99 parking spaces. The Bank proposes to provide 57 parking spaces with 30 such spaces parking spaces adjacent to the Bank building and 27 parking spaces (including 4 tandem spaces) at 0 Garden Street. This will require relief from the following:

Section 5.1.2 - Required Parking: There are 57 parking spaces (including 4 tandem spaces in the Garden Street Parking Lot) available to serve the Subject Property. The Bank requires 99 parking spaces. Therefore, the Bank requires and requests a waiver of 42 parking spaces. As reported above, the Bank currently has been granted a 32 parking space waiver (and is leasing spaces in the municipal lot).

Section 5.1.3(b) - Loading Requirements: The Bank requests relief from the requirement of providing off-street loading facilities. There are currently no such facilities at the subject property and this has not been problematic for Bank activity.

Section 5.1.3(h) – Parking Space Layout: The Bank requests relief from the parking space layout requirement to the extent necessary for the proposed tandem spaces in the Garden Street Parking Lot and the parking spaces located on both Bank and Town property

Sections 5.1.3(i), (j), (k), (l) and (m) – Aisle, Parking Setbacks, Landscaped Areas, Trees, and Location: The Bank requests relief from the requirements of parking setbacks, landscaped areas, and trees. Strict compliance with these requirements will reduce the number of parking spaces available on the site. This will allow the utilization of the Bank and Town lots as an integrated parking lot.

Section 3.2.2 – Location of Parking: The Bank proposes parking in separate lots including the Bank owned Garden Street Parking Lot. In addition, there are parking spaces proposed to traverse both Bank property and Town property. This will allow the maximization of parking spaces available to both the Bank and the Town.

B. DRIVE-UP TELLER BUILDING WITH DRIVE-UP ATM: As indicated above, the Project includes construction of a free-standing drive through teller building with a drive-up ATM. Recognizing the prohibition within the CB Zoning District, this building has been located within the CSB Zoning District. Under Section 3.2.2 of the Needham Zoning By-Law, the same are allowed in the CSB Zoning District by special permit either as “other accessory uses allowed incidental to lawful principle uses”

C. SITE PLAN REVIEW: The Bank requests that the Planning Board waive the submission of materials pertaining to the following:

Section 7.4.4(i) – Drainage: Given the relative small (321 sf) proposed new building (which is replacing an existing drive-up ATM), the Bank proposes to utilize the existing drainage.

Section 7.4.4(l) – Lighting: Given the relative small (321 sf) proposed new building (which is replacing an existing drive-up ATM), the Bank proposes to utilize current lighting of the parking lot which has been sufficient to date.

Section 7.4.4(m) – Loading and unloading facilities: There are no off-street loading facilities. The Bank has minimal needs for loading and unloading facilities and has experienced no problems to date.

Section 7.4.4(n) – Provisions for refuse removal: For security reasons, trash is removed from the Bank building after every business day and is not store in dumpsters or other external containers. Refuse removal will remain as currently in place.

Section 7.4.4(o) – Project traffic volumes: Applicant believes that the Project will make the Bank more efficient and addresses consumer banking habits. However, it is not anticipated that the Project will increase business or traffic generation. Simply stated, customers who now come into the Bank will be able to utilize the drive-up teller.

V. APPROVAL STANDARDS

Sections 5.1.1.5 and 5.1.1.6 of the Needham Zoning By-Law sets forth conditions which must be satisfied for issuance of a special permit to waive strict compliance with the above referenced Sections 5.1.2. and 5.1.3. The Bank respectfully submits that it satisfies these requirements as follows:

Replaces or substantially improves an existing building or site: The addition to the Bank will dramatically improve the Bank efficiency and functioning. The new drive-up building has been designed of an appropriate scale (i.e., it is only 321 square feet) and to blend with the existing building.

Promotes the goal of preserving and enhancing the CBD as a pedestrian – oriented local shopping and business district: It is noteworthy that a “bank” is a use allowed as of right in the CB and CSB Zoning Districts. The previous enhancements to the parking in the area and development of the pedestrian plaza a directly related to the Town’s goals and objectives for this area. This will not change by this Project

Incorporates the recommendations of the Design Review Board: The Project is simultaneously being submitted to and will be reviewed by the Design Review Board. The Bank is committed to continuing this working relationship with the Town as it meets with and considers the recommendations of the Design Review Board.

Demonstrates that it is providing the maximum number of off-street parking spaces practicable: Parking is a major and important aspect of this Project. Working cooperatively with the Town had previously resulted in the discontinuance of Eaton Square and has allowed for enhanced parking for the Bank and the Town. Again, this is not impacted by the Project.

IX. SITE PLAN REVIEW CRITERIA

Section 7.4.6 of the Needham Zoning By-Law sets forth the criteria the Town shall consider in conducting site plan review. The Bank respectfully submits that it satisfies the criteria as follows:

(a) Protection of adjoining premises against seriously detrimental uses by provision for surface water drainage, sound and sight buffers and preservation of views, light, and air;

The Project is minimal in terms of any new impacts. The major renovation is inside an existing building. The proposed new drive-up teller building is only 321 square feet. The existing drainage and landscaping will remain.

(b) Convenience and safety of vehicular and pedestrian movement within the site and on adjacent streets, the location of driveway openings in relation to traffic or to adjacent streets and, when necessary, compliance with other regulations for the handicapped, minors and the elderly;

As stated above, in light of the changes in consumer banking habits as well as the Applicant's allowance of workers to work remotely, no increase in traffic volumes is anticipated. Furthermore, the new drive-up teller building is small (321 square feet) and has been designed to allow easy access.

(c) Adequacy of the arrangement of parking and loading spaces in relation to the proposed uses of the premises;

The Bank does not require loading facilities. The site layout has been arranged to optimize the parking configuration.

(d) Adequacy of the methods of disposal of refuse and other wastes resulting from the uses permitted on the site;

The Bank does not require exterior storage of refuse for removal. The Bank maintains contracts for proper handling, shredding, removal, and disposal of all waster materials.

(e) Relationship of structures and open spaces to the natural landscape, existing buildings and other community assets in the area and compliance with other requirements of this By-Law; and

The existing site is fully developed. The Project does not require destruction of existing undeveloped sites and is a sustainable design practice. Setbacks to other existing buildings and community assets are being maintained.

(f) Mitigation of adverse impacts on the Town's resources including the effect on the Town's water supply and distribution system, sewer collection and treatment, fire protection, and streets; and may require when acting as the Special Permit Granting Authority or recommend in the case of minor projects, when the Board of Appeals is acting as the Special Permit Granting Authority, such appropriate conditions, limitations, and safeguards necessary to assure the project meets the criteria of a through f.

The Project consists of a small (321 square foot) drive-up building replacing an existing drive-up ATM as well as expansion of floor area interior to the Bank building. As such, the Project does not constitute a new significant strain on the Town's resources. The grant of (or modification to existing) Special Permits will allow the Bank to continue to provide service to the community in more efficient and effective manner. The Bank is an established community business, a considerate neighbor, and will comply with all state and local conditions, limitations, and safeguards.

NEEDHAM BANK

RENOVATION AND NEW ACCESSORY DRIVE-THRU

**1063 GREAT PLAIN AVE
NEEDHAM, MA 02492**



LOCUS MAP

(NOT TO SCALE)



OWNER/APPLICANT:

NEEDHAM BANK, JIM WHITE
1063 GREAT PLAIN AVENUE
NEEDHAM, MA 02492

ASSESSOR'S ID:

MAP 51
PARCEL 86
BOOK 28389
PAGE 388

ISSUED FOR:

SUBMITTAL DATE: 06/14/22
REVISED DATE:

PROJECT TEAM:

CIVIL ENGINEER:
HIGHPOINT ENGINEERING
980 WASHINGTON STREET, SUITE 216
DEDHAM, MA 02026
TEL: (781) 713-3401
ATTN: DEREK REDGATE, P.E.
www.highpointeng.com

LAND SURVEYOR:

FIELD RESOURCES, INC.
281 CHESTNUT STREET
NEEDHAM, MA 02492
TEL: (781)-444-5936

ARCHITECT:

STUDIO Q
301 HIGHLAND AVE
WATERBURY, CT 06708
TEL: (203) 725-6335
ATTN: RON QUICQUARO



DEDHAM EXECUTIVE CENTER
980 WASHINGTON ST., SUITE 216
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CLIENT:

CONSULTANT:

SEAL



NEEDHAM BANK
RENOVATION & NEW ACCESSORY DRIVE-THRU
1063 GREAT PLAIN AVE
NEEDHAM, MA

OWNER/APPLICANT: NEEDHAM BANK

[illegible][illegible]

ISSUE TYPE:

PERMIT

ISSUE DATE:
06/14/22

PROJECT NUMBER:
22021

DRAWN BY: NCK

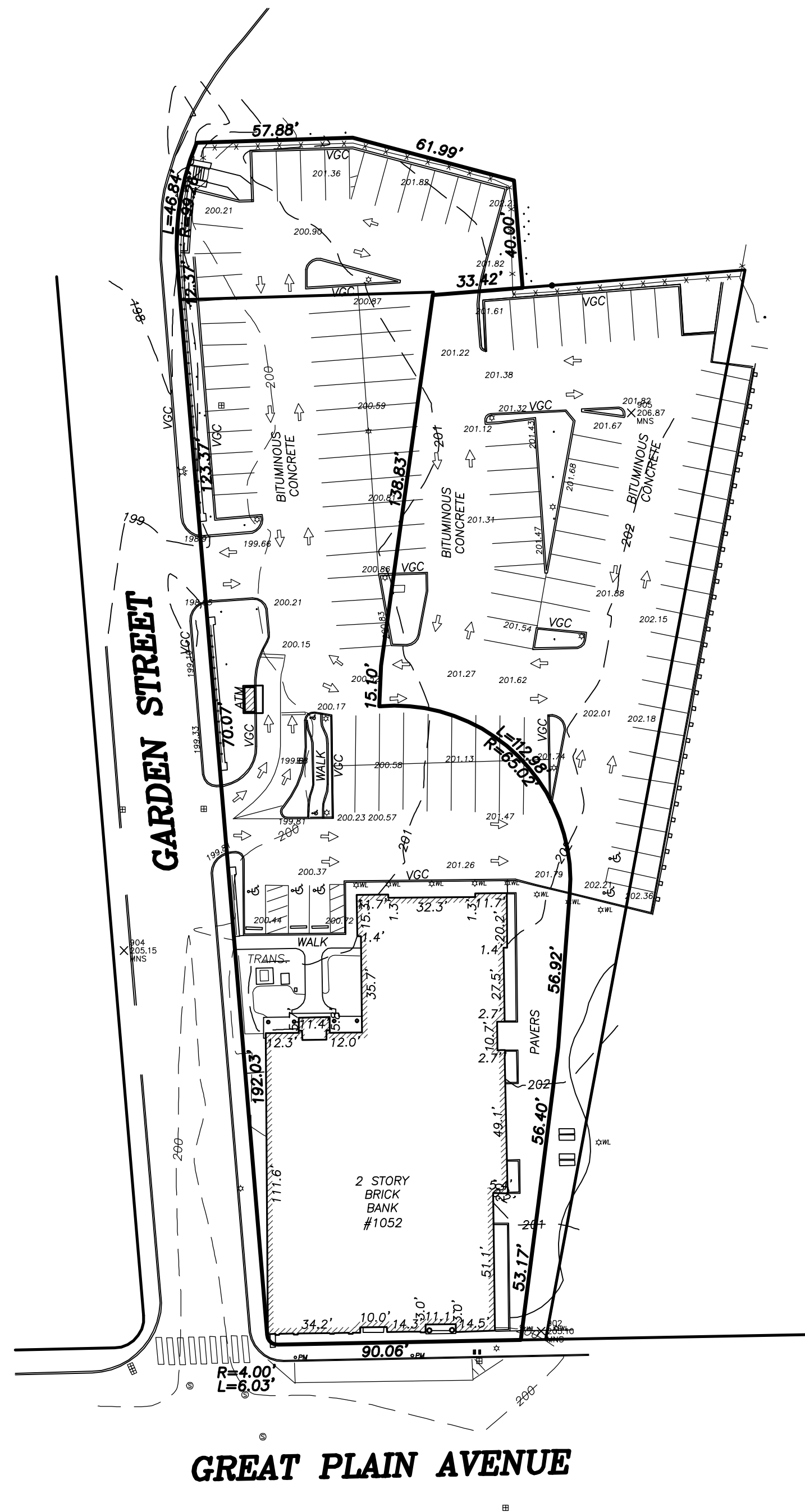
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TITLE SHEET

SHEET NUMBER:

G100

ISSUED FOR:



EXISTING CONDITIONS
1052 GREAT PLAIN AVENUE
NEEDHAM, MASS.

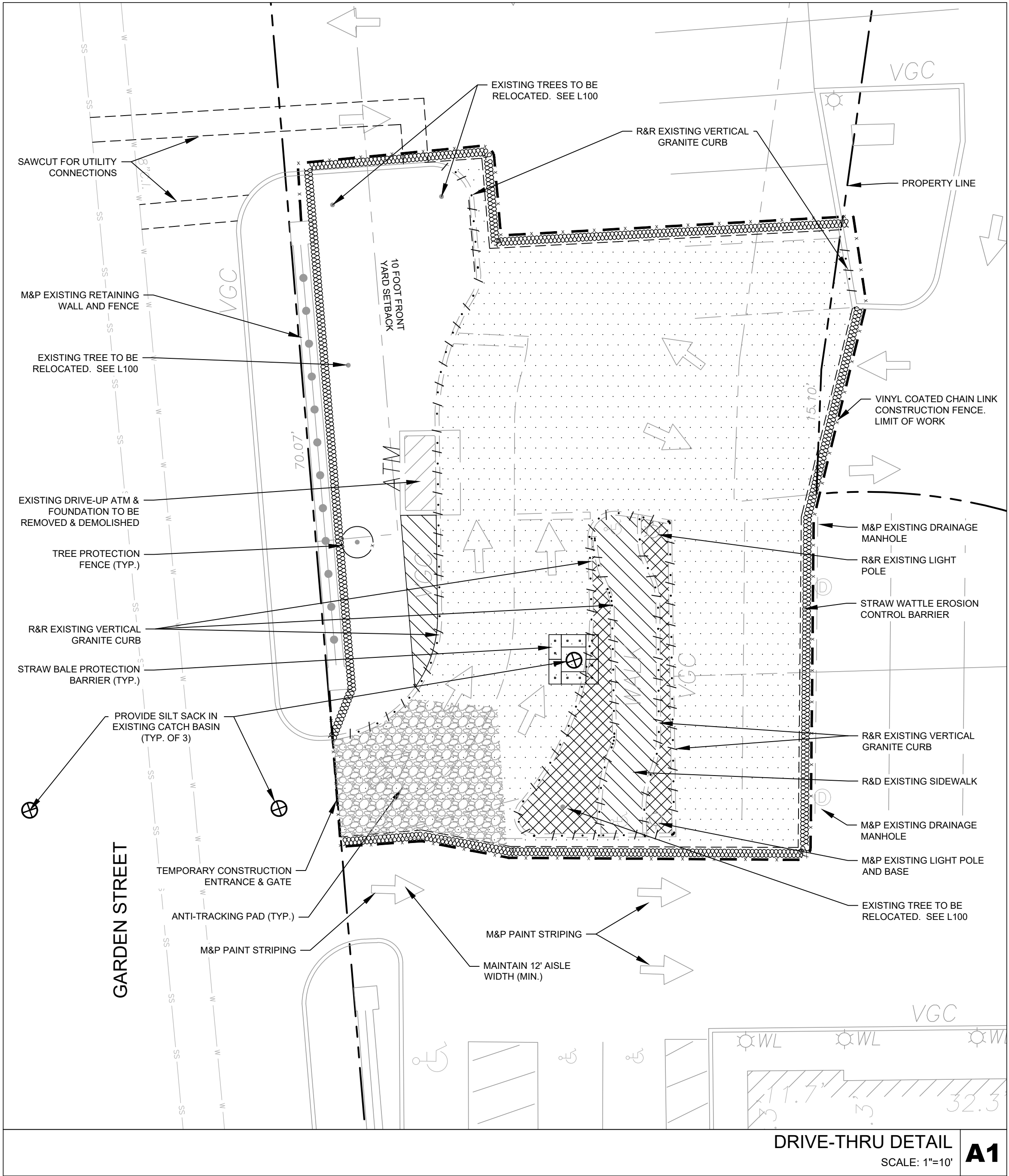
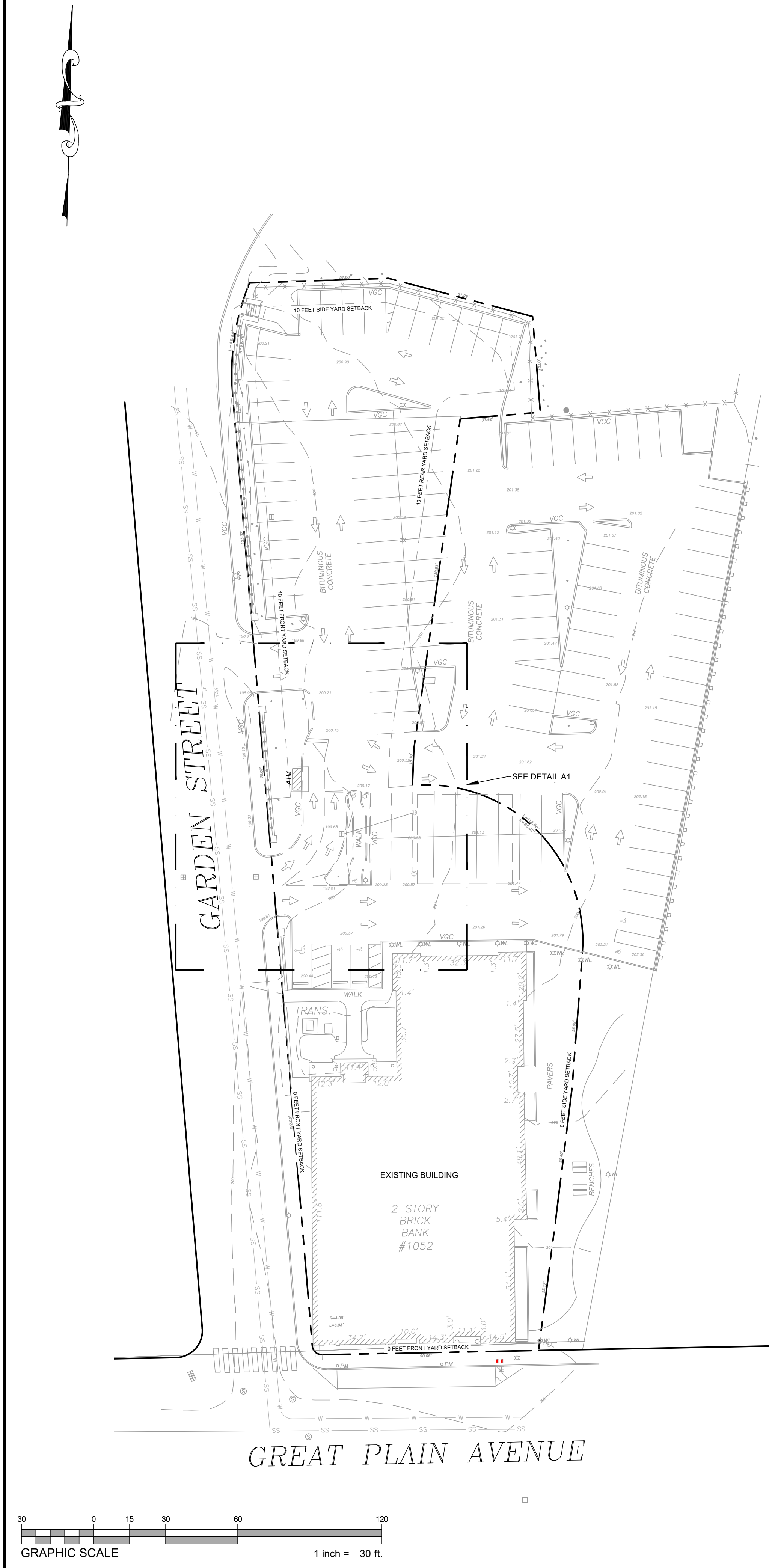
Field Resources, Inc.
LAND SURVEYORS

JOB NO. 057-22

JUNE 13, 2022 SCALE 1"=40'
P.O. BOX 324 281 CHESTNUT ST.
AUBURN, MA NEEDHAM, MA.
508 832 4332 781 444 5936
fieldresources@hotmail.com

REVISED

057-22



EROSION AND SEDIMENTATION CONTROL NOTES

1. UNLESS DIRECTED OTHERWISE, ALL EXISTING TURF OR VEGETATED AREAS WITHIN THE PROPOSED LIMITS OF WORK FOR EXCAVATION, GRADING, OR IMPROVEMENT SHALL BE CLEARED AND GRUBBED. WITHIN THE CLEARING AND GRUBBING AREA, REMOVE ALL TREES, SHRUBS AND ROOTS UNLESS DESIGNATED OTHERWISE. CLEARING SHALL INCLUDE THE FELLING, CUTTING AND OFF-SITE DISPOSAL OF ALL TREES, SHRUBS, STUMPS AND VEGETATIVE DEBRIS PRODUCED THROUGH THE CLEARING OPERATIONS. TREES DESIGNATED FOR REMOVAL SHALL BE TAGGED BY CONTRACTOR AND APPROVED BY OWNER'S REPRESENTATIVE PRIOR TO COMMENCEMENT OF CONSTRUCTION.
2. THE LOCATION OF EROSION CONTROL BARRIERS SHOWN ON DRAWINGS ARE INTENDED TO BE MINIMUM REQUIREMENTS AND A GUIDE FOR THE PLACEMENT OF THESE BARRIERS. OTHER MEASURES MAY BE WARRANTED BASED UPON EXPERIENCE AT THE SITE. WHEN NO SEDIMENTATION CONTROL SYSTEM IS SHOWN ON THE DRAWING, THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A SYSTEM TO PREVENT SILTATION OR POLLUTION TO ADJACENT PROPERTY. THE SYSTEMS SHOWN SHALL NOT RELIEVE THE CONTRACTOR FROM THE RESPONSIBILITY OF PLACING ADDITIONAL BARRIERS OR REPLACING BARRIERS AS REQUIRED BY SITE CONDITIONS. THE IMPLEMENTATION, MAINTENANCE, REPLACEMENT AND ADDITIONS TO THESE SYSTEMS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. AS CONSTRUCTION PROGRESSES AND SEASONAL CONDITIONS DICTATE, MORE SILTATION CONTROL FACILITIES MAY BE REQUIRED. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ADDRESS NEW CONDITIONS THAT MAY BE CREATED. THE CONTRACTOR SHALL REGULARLY INSPECT THE PERIMETER OF THE PROPERTY TO CLEAN UP AND REMOVE LOOSE CONSTRUCTION DEBRIS. ALL DEMOLITION DEBRIS SHALL BE PROMPTLY REMOVED FROM THE SITE AND DISPOSED AT A LEGAL DUMP SITE. ALL TRUCKS LEAVING THE SITE SHALL BE COVERED.
3. PRIOR TO STARTING ANY WORK ON THE SITE, THE CONTRACTOR SHALL NOTIFY APPROPRIATE AGENCIES AND SHALL INSTALL EROSION CONTROL MEASURES AS SHOWN ON PLANS AND AS IDENTIFIED IN FEDERAL, STATE, AND LOCAL APPROVAL DOCUMENTS PERTAINING TO THIS PROJECT.
4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO INSTITUTE EROSION CONTROL MEASURES ON AN AS NECESSARY BASIS, SUCH THAT EXCESSIVE SOIL EROSION DOES NOT OCCUR. MEASURES SHALL INCLUDE STRAW BALE DIKES AROUND DRAINAGE INLETS, MULCHING, AND PLANTING OF DISTURBED AREAS.
5. AN EROSION CONTROL BARRIER IS TO BE INSTALLED AT THE PROPOSED DOWN GRADIENT TOE OF SLOPE AT ALL LOCATIONS WHERE EARTHWORK IS PROPOSED.
6. DURING CONSTRUCTION THE EROSION CONTROL MEASURES SHALL BE INSPECTED ONCE PER WEEK AND WITHIN 24 HOURS OF ANY STORM EVENT GENERATING MORE THAN 1/4" OF RAINFALL. THE EROSION CONTROL MEASURES SHALL BE CLEANED REGULARLY AND ADJUSTED IF NECESSARY, TO ENSURE THAT NO SILT OR DEBRIS LEAVES THE SITE.
7. ALL POINTS OF CONSTRUCTION EGRESS OR INGRESS SHALL BE MAINTAINED TO PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC ROADS. ANY SEDIMENT TRACKED ONTO PUBLIC RIGHT-OF-WAYS SHALL BE SWEEPED AT THE END OF EACH WORKING DAY.
8. CONTRACTOR SHALL PREVENT DUST, SEDIMENT, AND DEBRIS FROM EXITING THE SITE AND BE RESPONSIBLE FOR CLEANUP, REPAIRS AND CORRECTIVE ACTION IF SUCH OCCURS. ANY DAMAGE RESULTING FROM CONSTRUCTION LOADS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO OWNER.
9. ANY WATER PUMPED FROM EXCAVATIONS WILL BE CONVEYED BY HOSE TO AN UPLAND AREA AND DISCHARGED INTO STRAW BALE CORRALS OR SEDIMENTATION BAGS.
10. ALL AREAS DISTURBED DURING CONSTRUCTION AND NOT SPECIFIED FOR PAVEMENT OR NOT LEFT IN A NATURAL CONDITION SHALL RECEIVE SIX (6) INCHES OF LOAM AND SEED.
11. AREAS NOT DISTURBED BY CONSTRUCTION SHALL BE LEFT NATURAL. CARE SHALL BE TAKEN TO PRESERVE EXISTING TREES, GROUND COVER AND OTHER NATURAL FEATURES WHENEVER POSSIBLE.
12. ANY DAMAGE OR DISTURBANCE OF EXISTING INFRASTRUCTURE CAUSED DURING CONSTRUCTION SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE TO ITS ORIGINAL CONDITION.
13. CONTRACTOR SHALL CONTROL STORMWATER RUNOFF DURING CONSTRUCTION TO PREVENT ADVERSE IMPACTS TO OFF SITE AREAS, AND SHALL BE RESPONSIBLE TO REPAIR RESULTING DAMAGES, IF ANY, AT NO COST TO OWNER.
14. ALL STOCKPILE AREAS SHALL BE LOCATED WITHIN LIMIT OF WORK LINE AND STABILIZED TO PREVENT EROSION. REFER TO STORMWATER POLLUTION PREVENTION PLAN FOR STABILIZATION REQUIREMENTS.
15. ALL DEBRIS GENERATED DURING SITE PREPARATION ACTIVITIES SHALL BE LEGALLY DISPOSED OF OFF-SITE.
16. SITE ELEMENTS TO REMAIN MUST BE PROTECTED FOR DURATION OF PROJECT.
17. ALL TOPSOIL ENCOUNTERED WITHIN WORK AREA SHALL BE STRIPPED TO ITS FULL DEPTH AND STOCKPILED FOR REUSE. EXCESS TOPSOIL SHALL BE DISPOSED OF ON-SITE AS DIRECTED BY OWNER. TOPSOIL PILES SHALL REMAIN SEGREGATED FROM EXCAVATED SUBSURFACE SOIL MATERIALS.
18. DUST SHALL BE CONTROLLED BY SPRINKLING OR OTHER APPROVED METHODS AS NECESSARY, OR AS DIRECTED BY THE OWNER OR HIS REPRESENTATIVE.
19. LOAMING AND SEEDING OR MULCHING OF NON-PAVEMENT AREAS SHALL TAKE PLACE AS SOON AS PRACTICABLE.
20. STRAW WATTLES, STRAW BALES, SILT FENCE OR OTHER SILTATION CONTROLS SHALL BE MAINTAINED IN A SATISFACTORY CONDITION UNTIL CONSTRUCTION IS COMPLETED AND THE POTENTIAL FOR ON-SITE EROSION HAS PASSED.
21. AT THE END OF CONSTRUCTION, THE CONTRACTOR SHALL REMOVE ALL CONSTRUCTION DEBRIS AND SURPLUS MATERIALS FROM THE SITE. A THOROUGH INSPECTION OF THE WORK PERIMETER IS TO BE MADE AND ALL DISCARDED MATERIALS, BLOWN OR WATER CARRIED DEBRIS, SHALL BE COLLECTED AND REMOVED FROM THE SITE. THE CONTRACTOR SHALL NOT REMOVE ANY SILTATION CONTROLS UNTIL AUTHORIZED (IN WRITING) BY THE OWNER OR OWNER'S REPRESENTATIVE.
22. AT THE END OF CONSTRUCTION ALL DRAINAGE STRUCTURES ARE TO BE CLEANED OF SILT, STONES AND OTHER DEBRIS. EROSION CONTROL BARRIERS ARE TO BE REMOVED AND DISPOSED OF IN ACCORDANCE TO LOCAL REQUIREMENTS.

SYMBOL LEGEND	
	FILTER SOCK EROSION CONTROL BARRIER
	SILTSACK CATCH BASIN INSERT
	VINYL COATED CHAIN LINK CONSTRUCTION FENCE
	R&D EXISTING CONCRETE PAD
	R&D PAVEMENT
	TOP SOIL REMOVAL
	REMOVE & DISPOSE EXISTING OBJECT
	MAINTAIN AND PROTECT
	REMOVE AND DISPOSE
	REMOVE AND RESET

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380 WASHINGTON ST., SUITE 216
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CLIENT:

CONSULTANT:

SEAL

NEEDHAM BANK
RENOVATION & NEW ACCESSORY DRIVE-THRU
1063 GREAT PLAIN AVE
NEEDHAM, MA
OWNER/APPLICANT: NEEDHAM BANK

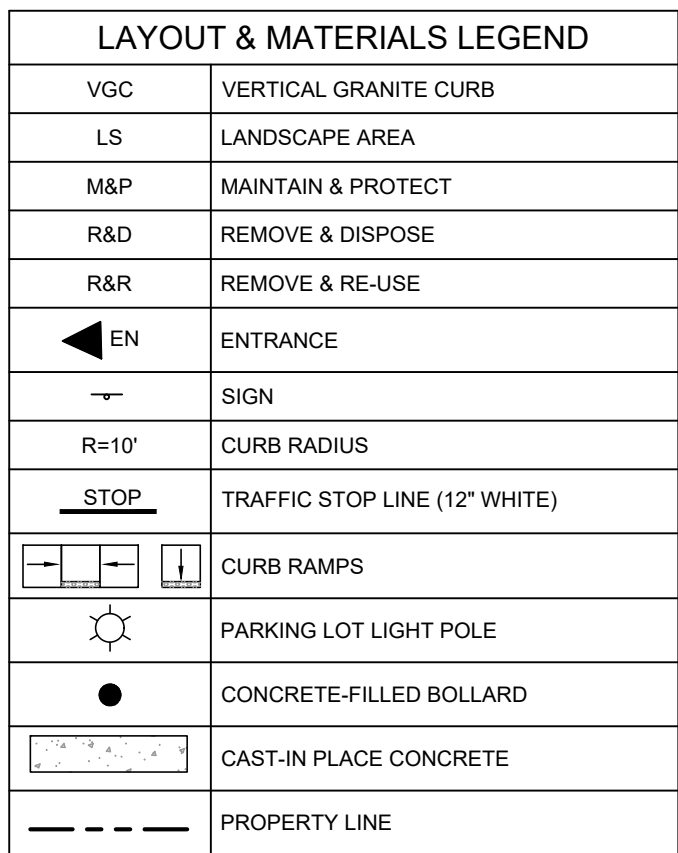
REV	DATE	DESCRIPTION
1	06/14/22	ISSUE TYPE: PERMIT
2	06/14/22	ISSUE DATE: 06/14/22
3	22021	PROJECT NUMBER: 22021

DRAWN BY: NCK
CHECKED BY: DBR
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SHEET TITLE:
SITE
PREPARATION &
EROSION
CONTROL PLAN

SHEET NUMBER:
C100

ISSUED FOR:



ZONING ORDINANCE DIMENSIONAL SUMMARY - 1063 GREAT PLAIN AVE					
ZONING DISTRICT: CHESTNUT STREET BUSINESS					
BUILDING DIMENSIONAL REQUIREMENTS					
CRITERIA	REQUIRED	EXISTING	PROPOSED	ZONING COMPLIANCE	
LOT AREA (MIN.)	10,000 SF	44,978 SF	NO CHANGE	YES	
LOT FRONTAGE (MIN.)	80 FT	540.77 FT	NO CHANGE	YES	
FAR (MAX.)	0.7	N/A	0.01	YES	
BUILDING HEIGHT (MAX)	2 ½ STORIES (35 FT)	N/A	1 STORY	YES	
BUILDING SETBACKS					
FRONT YARD ⁽¹⁾ (MIN)	10 FT	N/A	10 FT	YES	
SIDE YARD ⁽²⁾ (MIN)	10 FT	N/A	182.96 FT	YES	
REAR YARD ⁽³⁾ (MIN)	10 FT	N/A	NO CHANGE	YES	
PARKING SETBACKS					
FRONT YARD (MIN)	20 FT	N/A ⁽¹⁾	NO CHANGE	YES	
SIDE YARD (MIN)	4 FT	N/A ⁽³⁾	NO CHANGE	YES	
REAR YARD (MIN)	4 FT	N/A ⁽³⁾	NO CHANGE	YES	

1. IN THE GARDEN STREET OVERLAY DISTRICT, THE MINIMUM FRONT SETBACK SHALL BE THE LESSER OF 10 FEET FROM THE PROPERTY LINE OR THE AVERAGE SETBACK OF ADJUTING BUILDINGS WITHIN 100 FEET ON THE SAME SIDE OF THE STREET.

2. IN THE GARDEN STREET OVERLAY DISTRICT, THE MINIMUM REAR SETBACK SHALL BE 10 FEET WHERE A LOT ADJOINS A RESIDENTIAL DISTRICT, NON-CONFORMING EXISTING STRUCTURES SHALL BE PLACED AT LEAST FIFTY-FIVE FEET TO THE DISTRICT BOUNDARY AND TO THE DISTRICT BOUNDARY, AND WITHIN AN ALLEY, AT LEAST TWENTY-FIVE FEET TO THE DISTRICT BOUNDARY. SUCH SETBACKS SHALL BE SUITABLY LANDSCAPED AS SPECIFIED AT SECTION 4.4.8.5. THE REMAINDER OF THE SETBACK MAY BE USED FOR PARKING OR STORAGE USE NOT INCLUDING BUS STOPPING. THERE SHALL BE NO LOT CHANGES TO THE CHURCH LANE BUSINESS DISTRICT, ADJOINING THE COMMUTER RAIL RAILROAD RIGHT-OF-WAY. THE 50-FOOT BUFFER REQUIRED BY THE ORDINANCE SHALL NOT APPLY; HOWEVER, THERE SHALL BE A 10-FOOT LANDSCAPE BUFFER.

3. ALL OTHER SETBACK REQUIREMENTS APPROVED AS PART OF SPECIAL PERMITS ON SEPTEMBER 10, 2008, DEED BOOK 26076 PAGE 564.

ZONING ORDINANCE DIMENSIONAL SUMMARY - 1063 GREAT PLAIN AVE				
ZONING DISTRICT: CENTER BUSINESS				
USE: BANK				
BUILDING DIMENSIONAL REQUIREMENTS				
CRITERIA	REQUIRED	EXISTING	PROPOSED	ZONING COMPLIANCE
LOT AREA (MIN.)	10,000 SF	44,978 SF	NO CHANGE	YES
LOT FRONTAGE (MIN.)	80 FT	540.77 FT	NO CHANGE	YES
FAR (MAX.)	1.0	0.769	0.806	YES
BUILDING HEIGHT (MAX)	2 ½ STORIES (35 FT)	2 ½ STORIES	NO CHANGE	YES
BUILDING SETBACKS				
FRONT YARD (MIN)	0 FT ⁽¹⁾	0 FT	NO CHANGE	YES
FRONT YARD (MAX)	3 FT OR AVG. ⁽²⁾	AVG.	NO CHANGE	YES
SIDE YARD ⁽³⁾ (MIN)	-	N/A	NO CHANGE	YES
REAR YARD ⁽³⁾ (MIN)	-	N/A	NO CHANGE	YES
PARKING SETBACKS				
FRONT YARD (MIN)	20 FT	N/A ⁽⁴⁾	NO CHANGE	YES
SIDE YARD (MIN)	4 FT	N/A ⁽⁴⁾	NO CHANGE	YES
REAR YARD (MIN)	4 FT	N/A ⁽⁴⁾	NO CHANGE	YES

1. MINIMUM FRONT SETBACK IN THE NEEHDAM CENTER OVERLAY DISTRICT IS 60 FEET FROM THE PROPERTY LINE.
2. IN THE CENTER BUSINESS DISTRICT, THERE SHALL BE A FRONT SETBACK OF THREE (3) FEET OR A SETBACK CONSISTENT WITH THE AVERAGE OF THE SETBACKS OF THE TWO ADJACENT BUILDINGS, WHICHEVER IS SMALLER.
3. WHERE A LOT ADJAINS A RESIDENTIAL DISTRICT, NO BUILDING OR STRUCTURE SHALL BE PLACED OR CONSTRUCTED WITHIN (50) FEET OF A RESIDENTIAL DISTRICT BOUNDARY, AND WITHIN SAID STRIP, THE TWENTY-FIVE (25) FEET CLOSEST TO THE DISTRICT BOUNDARY SHALL BE SUITABLY LANDSCAPED AS SPECIFIED AT SECTION 4.6.5. THE REMAINDER OF THE SETBACK MAY BE USED FOR AN ACCESSORY USE NOT INCLUDING A BUILDING OR STRUCTURE.
4. WAIVER FOR PARKING SETBACK REQUIREMENTS APPROVED AS PART OF SPECIAL PERMIT ON SEPTEMBER 10, 2008, DEED BOOK 26076 PAGE 564.

OFF STREET PARKING SUMMARY					
PARKING ALLOTMENT REQUIREMENTS					
	PARKING RATE	EXISTING	PROPOSED	REQUIRED	PROVIDED
BUILDING USE	REQUIRED	SIZE	SIZE	SPACES	SPACES
BANK & ACCESSORY	1 SP / 300 SF FLOOR AREA	24,007 SF	25,693 SF	86	57 ⁽¹⁾⁽²⁾
UTILITY/STORAGE	1 SP / 850 SF FLOOR AREA	10,573 SF	10,573 SF	13	57 ⁽¹⁾⁽²⁾
TOTAL				99	57 ⁽⁴⁾

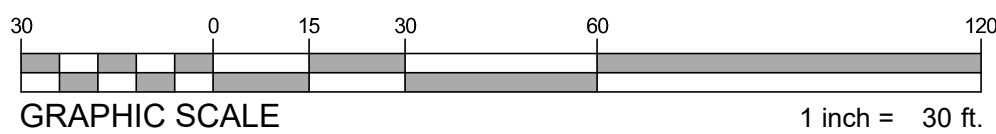
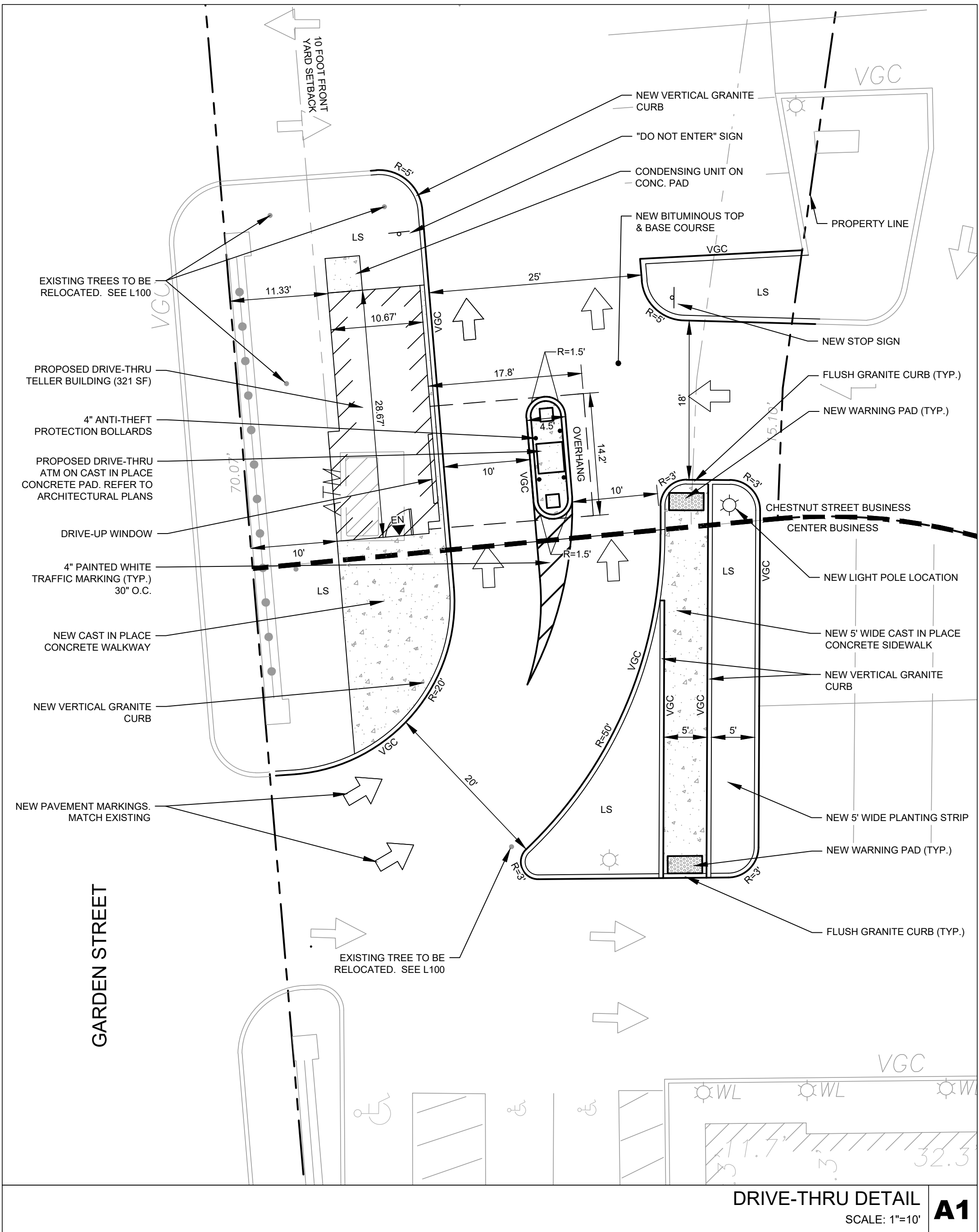
1. WAIVER FOR REDUCTION IN PARKING APPROVED AS PART OF SPECIAL PERMIT ON JUNE 12, 2012 FROM 94 SPACES TO 62 SPACES.
2. WAIVER FOR REDUCTION IN PARKING SPACES FROM 62 TO 57 REQUIRED AS PART OF SPECIAL PERMIT FILING.
3. ADDITIONAL PARKING FOR BANK EMPLOYEES PROVIDED AT A GARDEN STREET.
4. 30 SPACES WILL BE LOCATED AT 1063 GREAT PLAIN AVE AND 27 SPACES WILL BE LOCATED AT 168 GARDEN STREET.

OFF STREET PARKING DIMENSIONAL SUMMARY			
CRITERIA	REQUIRED	EXISTING	ZONING COMPLIANCE
STANDARD STALL WIDTH	9'	9'	YES
STANDARD STALL DEPTH	18'	18'	YES
DRIVEWAY WIDTH (TWO-WAY)	24'	30'	YES
DRIVEWAY WIDTH (ONE-WAY)	15'	N/A	YES

1. PROPOSED PARKING REDUCES ON-SITE PARKING BY 5 SPACES.

SITE LAYOUT AND MATERIALS NOTES

1. CONTRACTOR SHALL REPORT PLAN DISCREPANCIES TO THE OWNER OR OWNERS REPRESENTATIVE.
2. ACCESSIBLE ROUTES, PARKING SPACES, RAMPS, SIDEWALKS AND WALKWAYS SHALL BE CONSTRUCTED IN CONFORMANCE WITH FEDERAL AMERICANS WITH DISABILITIES ACT AND WITH STATE AND LOCAL LAWS AND REGULATIONS (WHICHEVER ARE MORE STRINGENT).
3. TRAFFIC SIGNAGE AND PAVEMENT MARKINGS SHALL CONFORM TO THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES AND THE TOWN OF NEEDHAM DESIGN STANDARDS.
4. EXISTING CURBS TO REMAIN AS SHOWN ARE ASSUMED TO BE IN SATISFACTORY CONDITION BUT ARE TO BE PARGED OR REPLACED IN KIND IN LOCATIONS OF DAMAGE.
5. ALL VERTICAL GRANITE CURB TO BE REMOVED AND RESET, SHALL BE SET IN ONE GENERAL LOCATION AND NOT RANDOMLY MIXED WITH NEW CURB.
6. INSTALL EXPANSION AND CONTROL JOINTS IN SIDEWALKS AT INTERVALS OF 5 FEET AND 25 FEET, RESPECTIVELY. PROVIDE BROOM FINISH IN TRANSVERSE DIRECTION ON ALL WALKS.
7. SIDEWALK WIDTHS INDICATED HEREON ARE MEASURED FROM BACK OF CURB TO BACK OF SIDEWALK, 6" WIDTH OF CURB NOT INCLUDED.
8. ALL CURB RADIUS DIMENSIONS SHOWN ARE MEASURED ALONG FACE OF CURB
9. ALL WORK CONDUCTED WITHIN PUBLIC RIGHT-OF-WAYS SHALL CONFORM TO THE TOWN OF NEEDHAM SPECIFICATIONS.
10. THE FOLLOWING LAYOUT CRITERIA SHALL CONTROL UNLESS OTHERWISE NOTED ON THE PLAN:
 - DIMENSIONS FROM BUILDING ARE FROM FACE OF BUILDING.
 - DIMENSIONS ARE TO FACE OF CURB AT GUTTER LINE.
 - DIMENSIONS ARE TO THE CENTER OF PAVEMENT MARKINGS.
11. ALL LINES AND DIMENSIONS AND TIES TO PROPERTY LINES ARE PERPENDICULAR TO THE PROPERTY LINE UNLESS OTHERWISE NOTED.
12. CONTRACTOR SHALL FURNISH AND SET ALL LINES AND GRADES REQUIRED AND PROTECT ALL PERMANENT BENCHMARKS OR MONUMENTS. DAMAGED MONUMENTS SHALL BE REPLACED BY A LICENSED SURVEYOR AT NO COST TO THE OWNER.
13. ALL CONCRETE WORK SHALL COMPLY WITH ACI301, "SPECIFICATION FOR STRUCTURAL CONCRETE," AND ACI 318R, UNLESS MODIFIED BY THE CONTRACT DOCUMENTS. COMPLY WITH CRSIS "MANUAL OF STANDARD PRACTICE" FOR FABRICATING, PLACING, AND SUPPORTING REINFORCED CONCRETE. COMPLY WITH ACI 308R FOR COLD WEATHER PROTECTION, AND FOLLOW RECOMMENDATIONS IN ACI 350R FOR HOT WEATHER PROTECTION DURING CURING. COMPLY WITH ACI 304 "GUIDE FOR MEASURING, MIXING, TRANSPORTING, AND PLACING CONCRETE."
14. ALL PROPOSED PAVEMENTS SHALL MEET THE LINE AND GRADE OF ADJACENT EXISTING PAVEMENT SURFACES AND SHALL BE TREATED WITH AN RS-1 TACK COAT AT POINT OF CONNECTION. ALL PATHWAY WIDTHS SHALL BE AS NOTED ON THE LAYOUT AND MATERIALS PLAN.
15. SAW-CUT EXISTING PAVEMENT WHERE NEW BITUMINOUS CONCRETE PAVEMENT IS TO COME IN CONTACT. PRIME COAT THE CUT EDGE PRIOR TO PLACEMENT.
16. CONTRACTOR(S) SHALL FAMILIARIZE THEMSELVES WITH ALL CONSTRUCTION DOCUMENTS, SPECIFICATIONS, AND ALL SITE CONDITIONS PRIOR TO CONSTRUCTION.



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RENOVATION & NEW ACCESSORY DRIVE-THRU
1063 GREAT PLAIN AVE
NEEDHAM, MA

OWNER/APPLICANT: NEEDHAM BANK

[illegible]

REV	DATE	DESCRIPTION
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PERMIT

ISSUE DATE-

06/14/22

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DRAWN BY: M

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SWEET TITLE:

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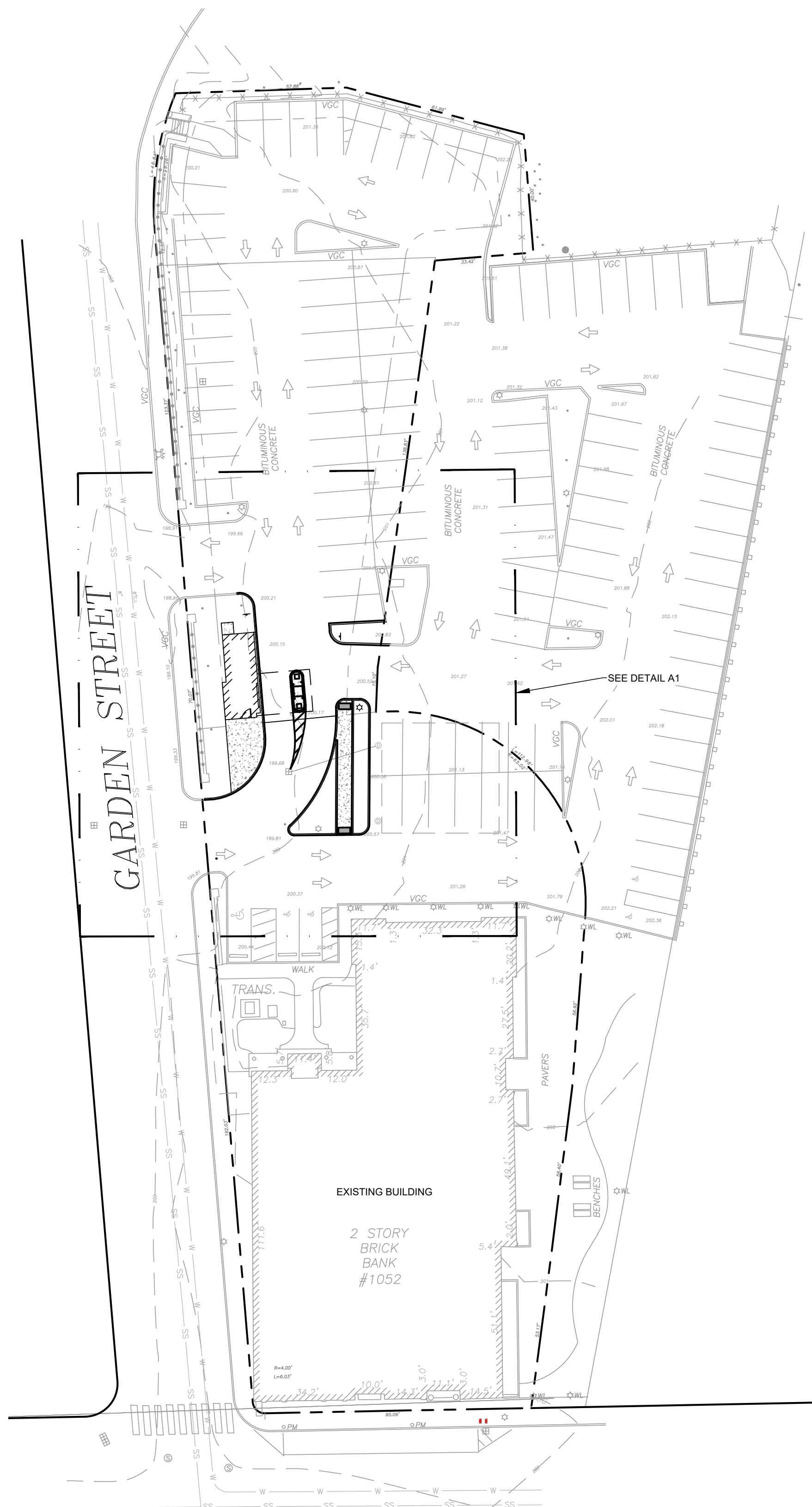
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GRAPHIC SCALE 1 inch = 30 ft.

UTILITY NOTES

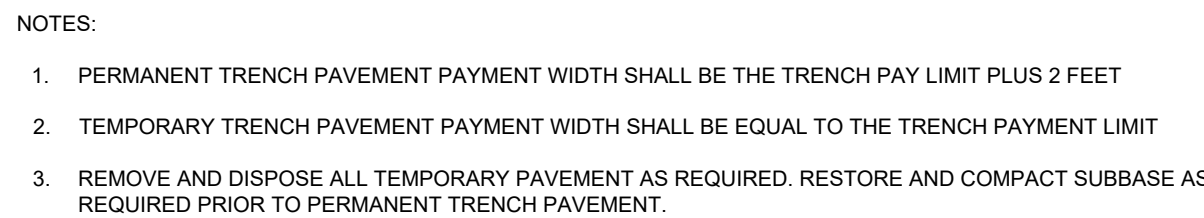
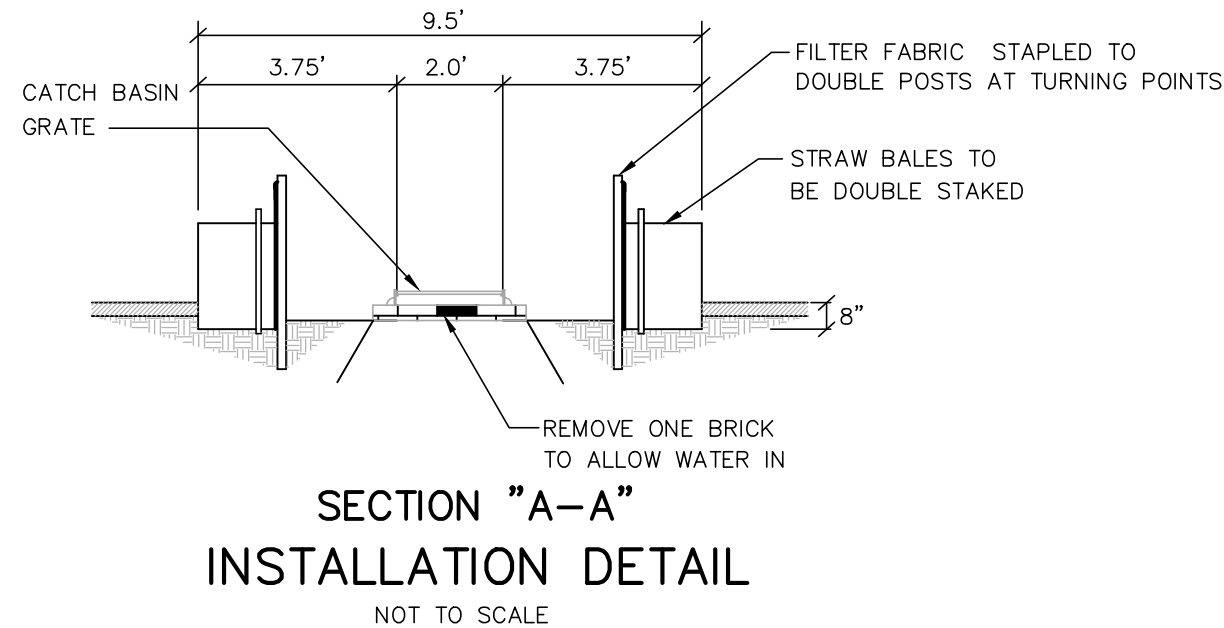
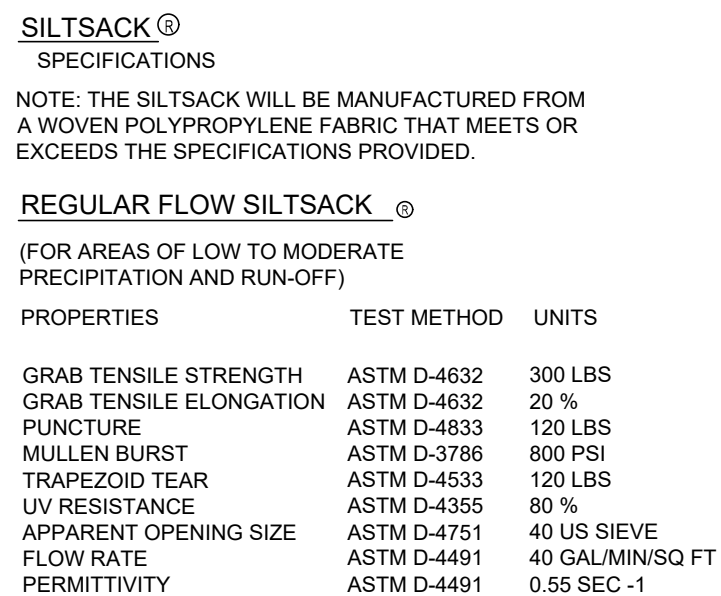
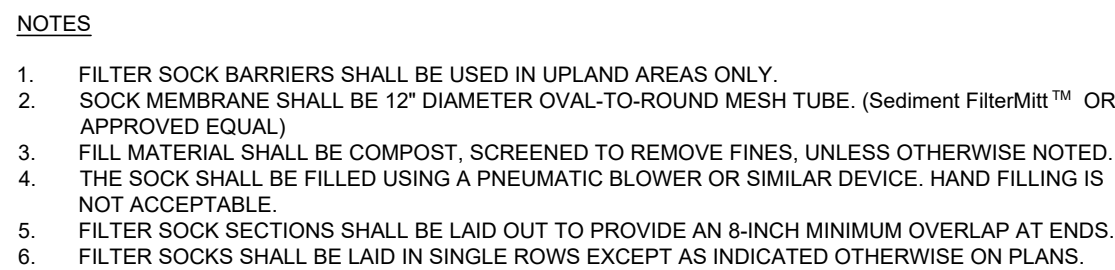
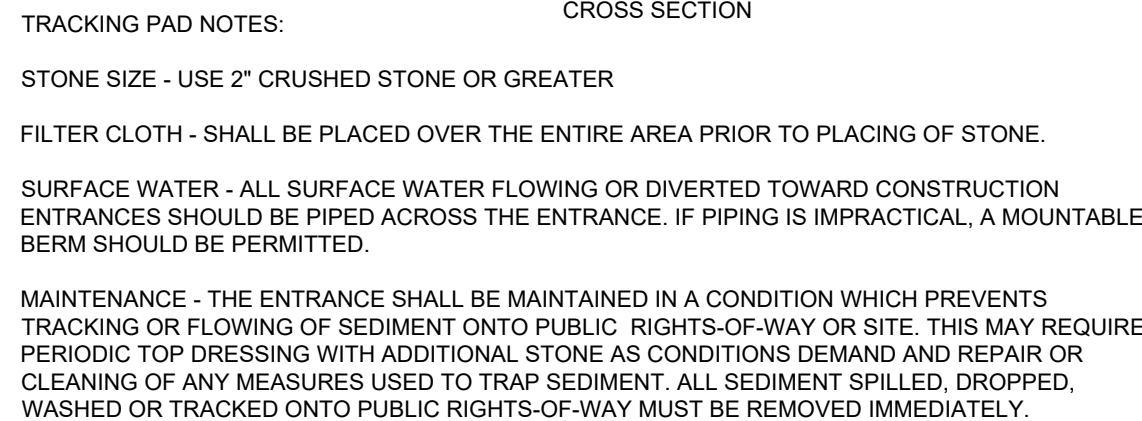
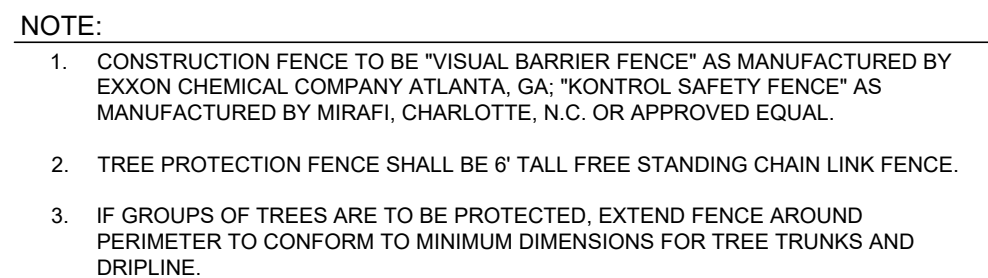
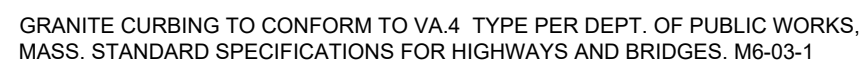
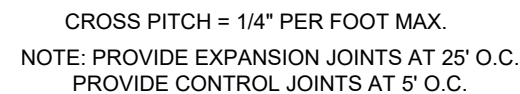
1. THE LOCATION OF UNDERGROUND UTILITIES AS REPRESENTED ON THESE PLANS IS BASED UPON EXISTING CONDITIONS PLANS AND INFORMATION AVAILABLE FROM THE TOWN OF NEEDHAM. ADDITIONAL INFORMATION MAY BE SUPPLEMENTED BY FIELD INVESTIGATIONS WHEREVER POSSIBLE. NO WARRANTY IS MADE AS TO THE ACCURACY OF THESE LOCATIONS OR THAT ALL UNDERGROUND UTILITIES ARE SHOWN. THE CONTRACTOR SHALL CONTACT DIG SAFE AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION. DIG SAFE TELEPHONE NUMBER IS 1-888-DIG-SAVE.
2. UNDERGROUND UTILITIES WERE COMPILED FROM AVAILABLE RECORD PLANS OF UTILITY COMPANIES AND PUBLIC AGENCIES AND ARE APPROXIMATE AND ASSUMED.
3. ALL PVC GRAVITY SANITARY SEWER PIPE SHALL BE SDR 35 WITH RUBBER RING JOINTS.
4. REFER TO PLUMBING PLANS FOR EXACT SIZE AND LOCATION OF SANITARY CONNECTIONS.
5. THE CONTRACTOR SHALL NOTIFY THE LOCAL MUNICIPAL DEPARTMENT OF PUBLIC WORKS AT LEAST 48 HOURS IN ADVANCE OF ANY REQUIRED INSPECTIONS.
6. UNDERGROUND INFRASTRUCTURE LOCATED IN THE PUBLIC WAY SHALL BE SUBJECT TO THE APPROVAL OF THE TOWN OF NEEDHAM DEPARTMENT OF PUBLIC WORKS.
7. NO LEDGE, BouldERS, OR OTHER UNYIELDING MATERIALS SHALL BE LEFT WITHIN 6" OF THE WATER AND SEWER IN THE TRENCH, NOR ARE THEY TO BE USED FOR BACKFILL FOR THE FIRST 12" ABOVE THE PIPES.
8. THE CONTRACTOR SHALL VERIFY THE LOCATION, SIZE AND DEPTH OF EXISTING UTILITIES PRIOR TO TAPPING OR CROSSING OR EXTENDING THEM. IF THE EXISTING WORKS CONFLICT WITH EXISTING UTILITIES, THE ENGINEER IS TO BE NOTIFIED PRIOR TO THE CONTRACTOR CONTINUING.
9. ALL MATERIALS FOR INSTALLATION OF ELECTRIC, GAS, AND DATA/TELECOM SHALL BE IN ACCORDANCE WITH LOCAL STATE AND UTILITY COMPANY STANDARDS AND REGULATIONS AS THEY APPLY.
10. ALL WATER STRUCTURES, FITTINGS, PIPES, ETC. SHALL BE IN ACCORDANCE WITH THE TOWN OF NEEDHAM WATER DEPARTMENT STANDARDS AND SPECIFICATIONS.
11. ALL SANITARY STRUCTURES, FITTINGS, PIPES, ETC. SHALL BE IN ACCORDANCE WITH THE TOWN OF NEEDHAM STANDARDS AND SPECIFICATIONS.
12. UNLESS OTHERWISE NOTED, ALL UTILITY TRENCHES ARE TO BE BACKFILLED WITH BANK RUN GRAVEL. NO STONES GREATER THAN 3" IN DIAMETER ARE TO BE USED WITHIN 12" OF THE PIPES. THE TRENCHES, WHEN UNDER PROPOSED PAVED AREAS, ARE TO BE MECHANICALLY COMPACTED IN 12" LIFTS.
13. CONTRACTOR SHALL PROTECT ALL UNDERGROUND DRAINAGE, SEWER AND UTILITY FACILITIES FROM EXCESSIVE VEHICULAR LOADS DURING CONSTRUCTION. ANY DAMAGE TO THESE FACILITIES RESULTING FROM TEMPORARY PEDESTRIAN CORRIDORS TO JERSEY BARRIERS AND THE LIKE, CONTRACTOR SHALL BE DONE UNTIL UTILITY COMPANIES ARE PROPERLY NOTIFIED IN ADVANCE.
14. EXCAVATION REQUIRED WITHIN THE PROXIMITY OF EXISTING UTILITY LINES SHALL BE DONE BY HAND. CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING UTILITY LINES OR STRUCTURES INCURRED DURING CONSTRUCTION OPERATIONS AT NO COST TO THE OWNER.
15. THE CONTRACTOR SHALL ALTER THE MASONRY OF THE TOP SECTION OF ALL EXISTING DRAINAGE STRUCTURES AS WELL AS ALL EXISTING MANHOLES TO ACCOMMODATE ALL WATER AND DRAINAGE FRAMES, GRATES AND BOXES TO THE PROPOSED FINISH SURFACE GRADE.
16. CONTRACTOR SHALL NOTIFY DIG-SAFE AT 8-1-1 AT LEAST 72 HOURS PRIOR TO ANY DEMOLITION OR CONSTRUCTION ACTIVITY. CONTRACTOR SHALL NOTIFY DESIGN ENGINEER IN WRITING OF ANY AND ALL DISCREPANCIES BETWEEN THESE PLANS AND OBSERVED EXISTING CONDITIONS PRIOR TO COMMENCING WORK.
17. ASPHALT TO BE DEMOLISHED SHALL BE REMOVED TO FULL DEPTH OF GRAVEL BASE UNLESS OTHERWISE INDICATED HEREON.
18. CONTRACTOR IS RESPONSIBLE FOR COORDINATING TEMPORARY ADJUSTMENTS TO PEDESTRIAN AND VEHICULAR CIRCULATION IN ALL PUBLIC WAYS DURING CONSTRUCTION, INCLUDING BUT NOT LIMITED TO RETRACTION OF TEMPORARY PEDESTRIAN CORRIDORS TO JERSEY BARRIERS AND THE LIKE. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE TOWN OF NEEDHAM ASSOCIATED WITH ALL WORK IN THE PUBLIC WAYS.
19. EXISTING CONDITIONS AND EXISTING SEWER INVERTS PROVIDED BY FIELD RESOURCES, INC.

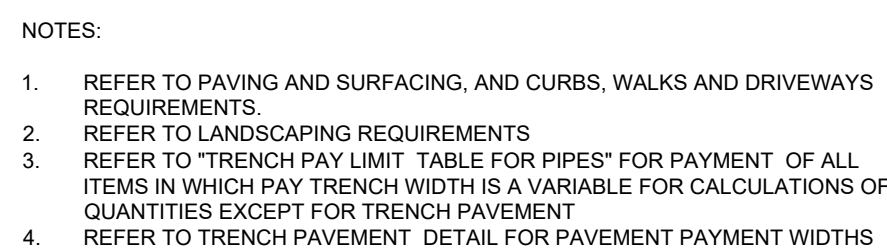
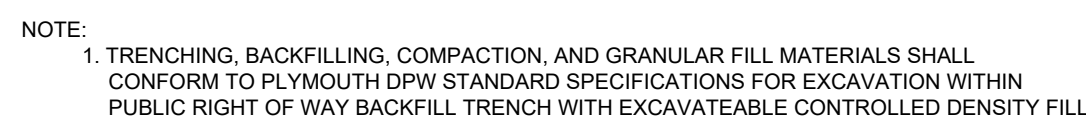
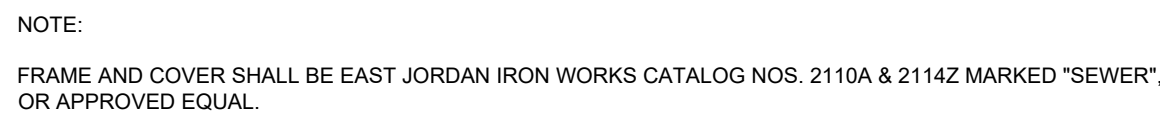
1. THE CONTRACTOR SHALL VERIFY EXISTING GRADES IN THE FIELD AND REPORT ANY DISCREPANCIES IMMEDIATELY TO THE OWNER OR HIS REPRESENTATIVE.
2. FILL MATERIAL SHALL BE AS SPECIFIED BY THE ARCHITECT/ENGINEER AND SELECTED FROM ON-SITE EXCAVATION MATERIAL WHERE POSSIBLE.
3. AT ALL LOCATIONS WHERE EXISTING CURB OR PAVEMENT ABUTS NEW CONSTRUCTION, THE EDGE OF THE EXISTING CURB OR PAVEMENT SHALL BE SAW CUT TO A CLEAN, SMOOTH EDGE. BLEND NEW CURBS AND EARTHWORK GRADUALLY INTO EXISTING BY MATCHING LINES. GRADES AND JOINTS, PITCH EVENLY BETWEEN SPOT GRADES. GRADE ALL AREAS TO DRAIN.
4. CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE AWAY FROM ALL BUILDING FOUNDATIONS, STRUCTURES AND PLANTING BEDS.
5. THE CONTRACTOR SHALL SCHEDULE HIS WORK TO ALLOW THE FINISHED SUBGRADE ELEVATIONS TO DRAIN PROPERLY WITHOUT PUDDING. SPECIFICALLY, ALLOW WATER TO ESCAPE WHERE PROPOSED CURB MAY RETAIN RUNOFF PRIOR TO APPLICATION OF THE FINISH SUBGRADE AND/OR SURFACE PAVING. PROVIDE TEMPORARY POSITIVE DRAINAGE AS REQUIRED.
6. PITCH EVENLY BETWEEN SPOT GRADES. GRADE ALL AREAS TO DRAIN. ALL PAVED AREAS MUST PITCH TO DRAIN AT A MINIMUM OF 1/8" PER FOOT UNLESS OTHERWISE SPECIFIED. ANY DISCREPANCIES NOT ALLOWING THIS MINIMUM PITCH SHALL BE REPORTED TO THE OWNER OR HIS REPRESENTATIVE PRIOR TO CONTINUING WORK.
7. ACCESSIBLE CURB RAMPS, RAMP, LANDINGS, WALKWAYS, CROSSWALKS, PATIOS/PLAZAS AND PARKING AREAS SHALL BE PER THE MASSACHUSETTS ARCHITECTURAL ACCESS BOARD (MAAB) AND THE AMERICANS WITH DISABILITIES ACT (ADA) ACCESSIBILITY REQUIREMENTS. WALKWAY AND CROSSWALK ALONG ACCESSIBLE ROUTE(S) SHALL HAVE 5% MAX LONGITUDINAL SLOPE AND 2% MAX CROSS SLOPE. LANGE, PATIO/PLAZAS, AND ACCESSIBLE PARKING SPACES SHALL BE 2% MAX IN ALL DIRECTIONS. RAMPS SHALL BE 1:12 MAXIMUM.
8. DURING THE PROGRESS OF THE WORK, THE CONTRACTOR MAY BE REQUIRED TO EXCAVATE ADDITIONAL TEST PITS FOR THE PURPOSE OF LOCATING UNDERGROUND UTILITIES OR STRUCTURES AS AN AID IN ESTABLISHING THE PRECISE LOCATION OF NEW WORK. THIS WORK IS TO BE PERFORMED AT NO ADDITIONAL COST TO THE OWNER. TEST PITS SHALL BE BACKFILLED, AS SOON AS THE DESIRED INFORMATION HAS BEEN OBTAINED.
9. PROTECT STRUCTURES, UTILITIES, SIDEWALKS, PAVEMENTS AND OTHER FACILITIES FROM DAMAGE CAUSED BY SETTLEMENT, LATERAL MOVEMENT, UNDERMINING, WASHOUT AND OTHER HAZARDS CREATED BY CONTRACTOR OPERATIONS.
10. EXISTING TREES AND SHRUBS OUTSIDE THE LIMITS OF GRADING SHALL BE REMOVED ONLY UPON PRIOR APPROVAL OF THE OWNER.
11. THE CONTRACTOR SHALL PREVENT SURFACE WATER AND SUBSURFACE OR GROUNDWATER FROM FLOWING INTO EXCAVATIONS OR EARTHWORK AREAS WHICH WOULD CAUSE FLOODING OF THE PROJECT SITE AND SURROUNDING AREA, OR SOFTENING OR LOOSENING OF THE SOIL AT EXCAVATION OR EARTHWORK SUB-GRADES.
12. PROTECT GRADED, FINISHED OR PAVED AREAS FROM DAMAGE AND KEEP THEM FREE OF TRASH AND DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS. REPAIR AND RE-ESTABLISH GRASSES IN SETTLED, ERODED AND RUINED AREAS.
13. PAVEMENT, LAWN OR PLANTING AREAS EXCAVATED DURING UTILITY CONSTRUCTION, WHETHER ON THE SITE OR ADJACENT PROPERTIES, SHALL BE RESTORED AND MATCHED WITH EXACTLY THE SAME MATERIALS AND TOLERANCES AS PRIOR TO DISRUPTION, AT NO ADDITIONAL COST TO THE OWNER, OR ADJACENT PROPERTY OWNERS.
14. AT THE END OF CONSTRUCTION, AFTER ALL DISTURBED AREAS HAVE BEEN STABILIZED, THE CONTRACTOR SHALL CLEAN THE SUMPS OF ALL CATCH BASINS AND THE INVERTS OF ALL DRAIN MANHOLES.

EXISTING IMPERVIOUS AREA WITHIN LIMIT OF WORK	=3,210 SF
PROPOSED IMPERVIOUS AREA WITHIN LIMIT OF WORK	=3,136 SF
CHANGE IN IMPERVIOUS AREA WITHIN LIMIT OF WORK	=74 SF (DECREASE)

PER SECTION 7.4.2 OF NEEDHAM'S STORM WATER MANAGEMENT POLICY, "APPLICATIONS FOR ADDITION TO A COMMERCIAL STRUCTURE, PARKING LOT, OR ROADWAY THAT IS LESS THAN 1,500 S.F." DO NOT REQUIRE A STORM WATER MANAGEMENT & EROSION CONTROL PLAN WHEN PROPOSED IN AN AREA OUT IN THE JURISDICTION OF THE CONSERVATION COMMISSION.

ISSUED FOR

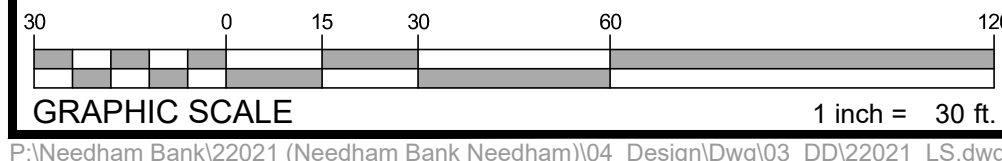




OWNER/APPLICANT: NEEDHAM BANK

CONSTRUCTION DETAILS

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C401



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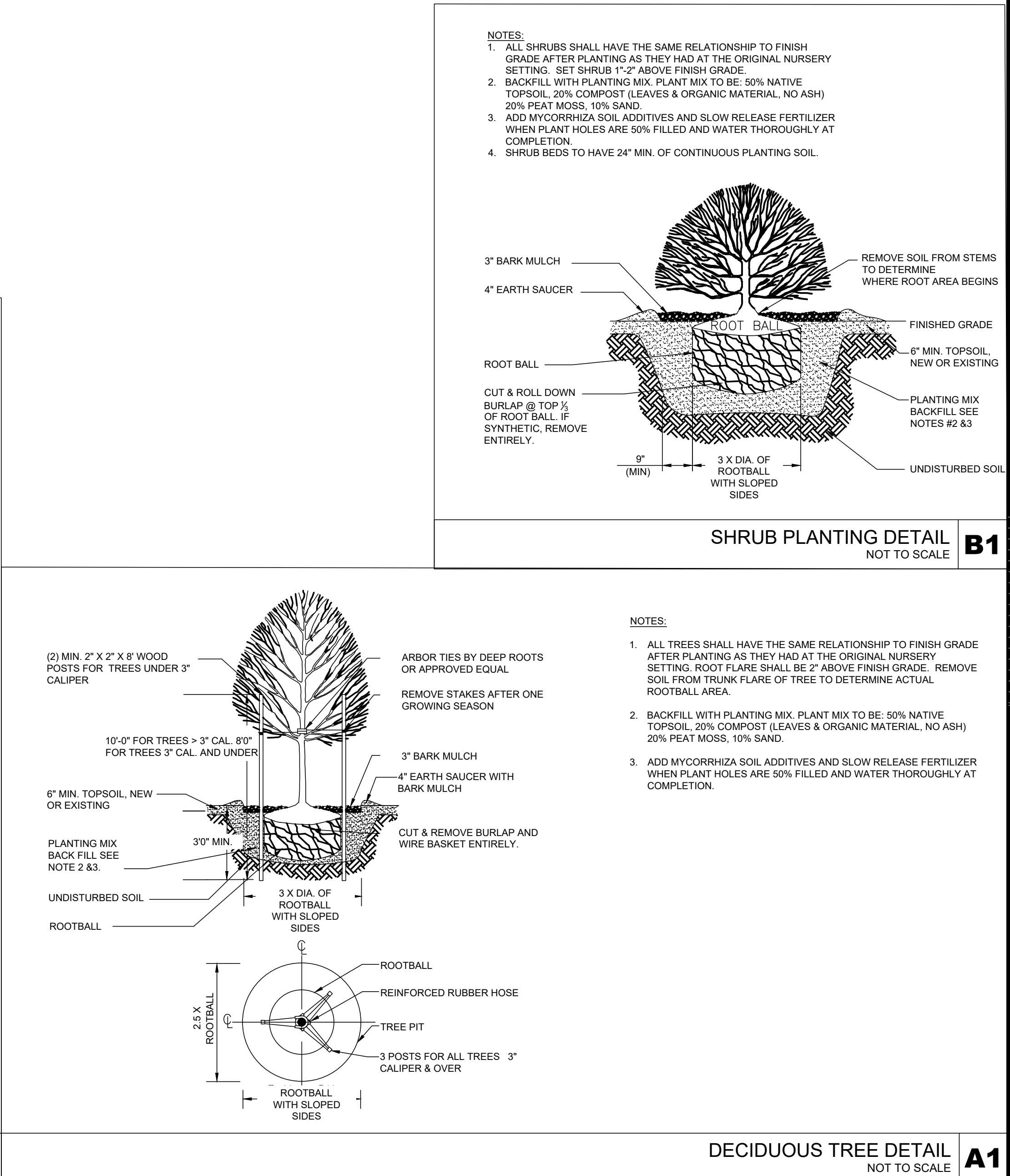
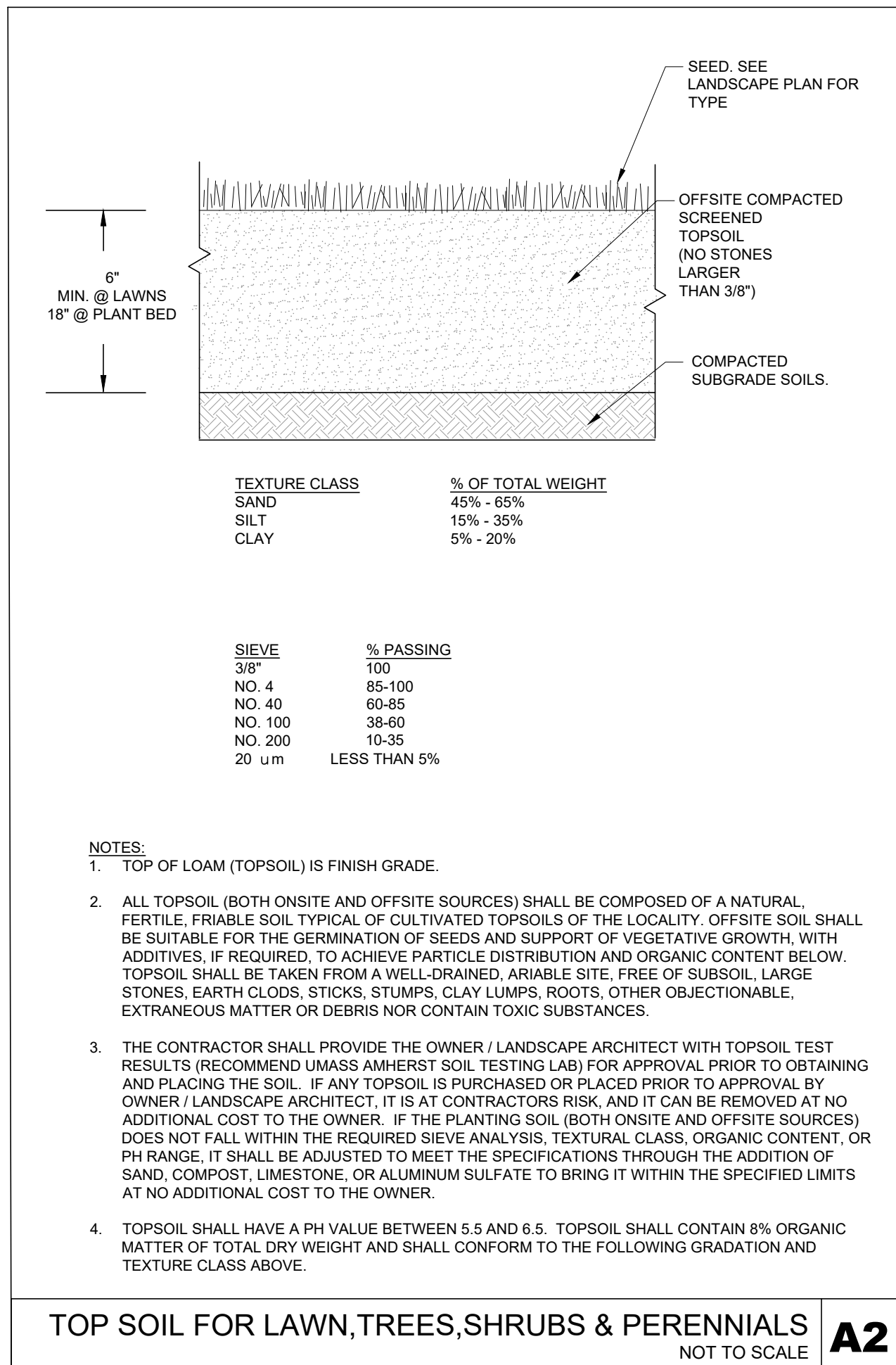
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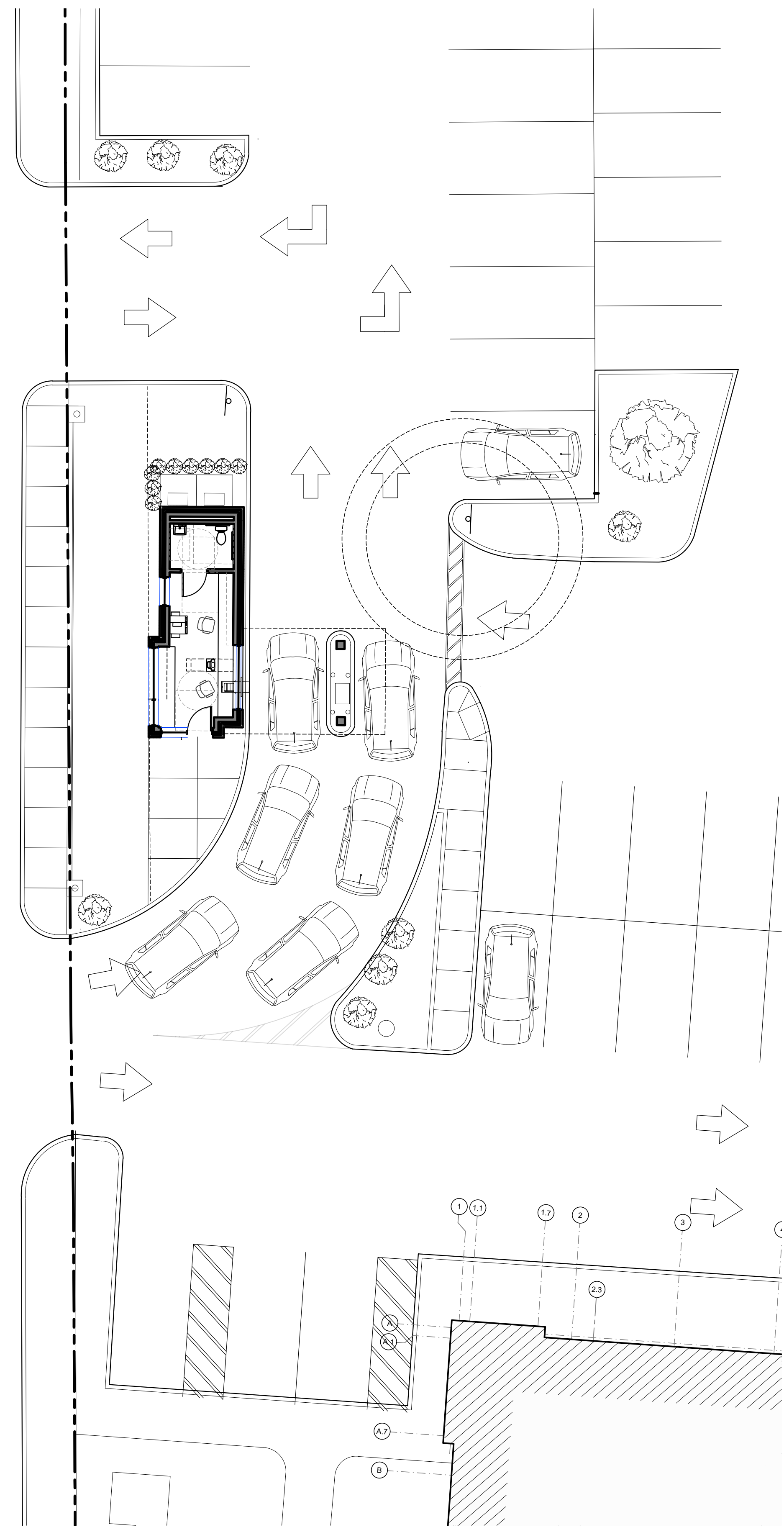
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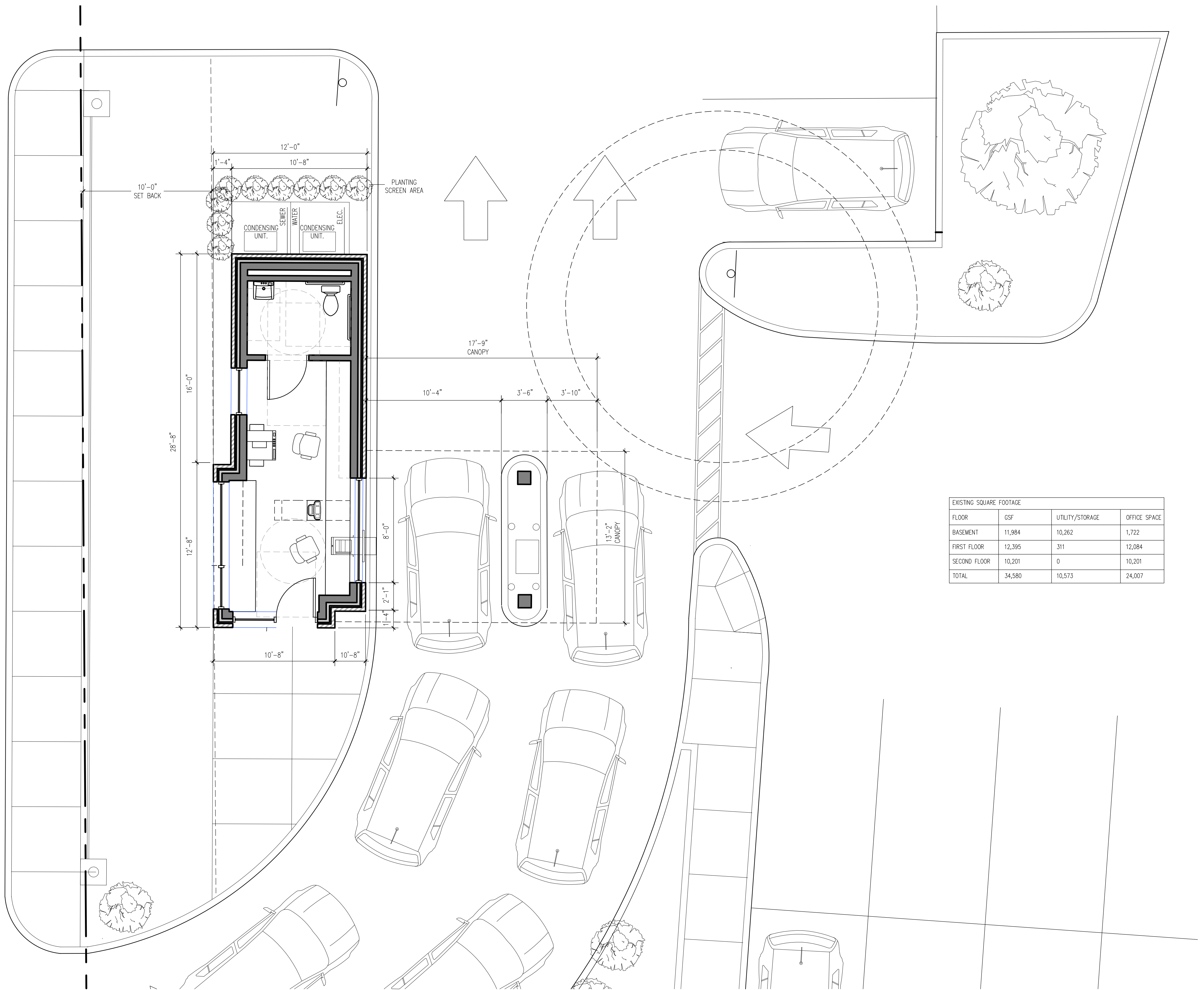
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RENOVATION & NEW ACCESSORY DRIVE-THRU
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NEEDHAM, MA

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[illegible]



2 SITE PLAN
SCALE: 1/20



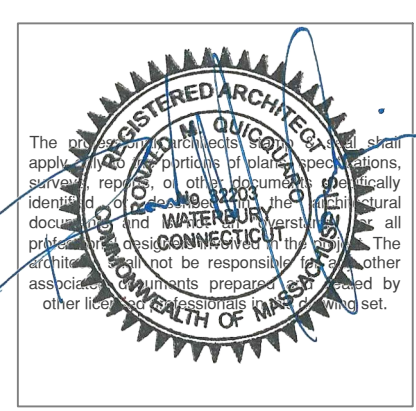
1 FLOOR PLAN
SCALE: 1/4" = 1'-0"

EXISTING SQUARE FOOTAGE			
FLOOR	GSF	UTILITY/STORAGE	OFFICE SPACE
BASEMENT	11,984	10,262	1,722
FIRST FLOOR	12,395	311	12,084
SECOND FLOOR	10,201	0	10,201
TOTAL	34,580	10,573	24,007

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1450 Main Street
East Hartford, CT 06108

STUDIO Q
architecture
DESIGN | PLANNING | INTERIORS

T (203) 725-6335
786 Chase Parkway
Waterbury, CT 06708



INTERIOR & EXTERIOR RENOVATIONS FOR:

NB Needham Bank
Build something.

1063 GREAT PLAIN AVE.

NEEDHAM, MA.

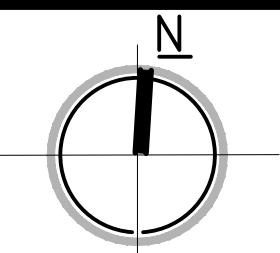
Issuances: 6.1.22 SITE PLAN SUBMISSION

Date: JUNE 1, 2022

Scale: 1/4" = 1'-0"

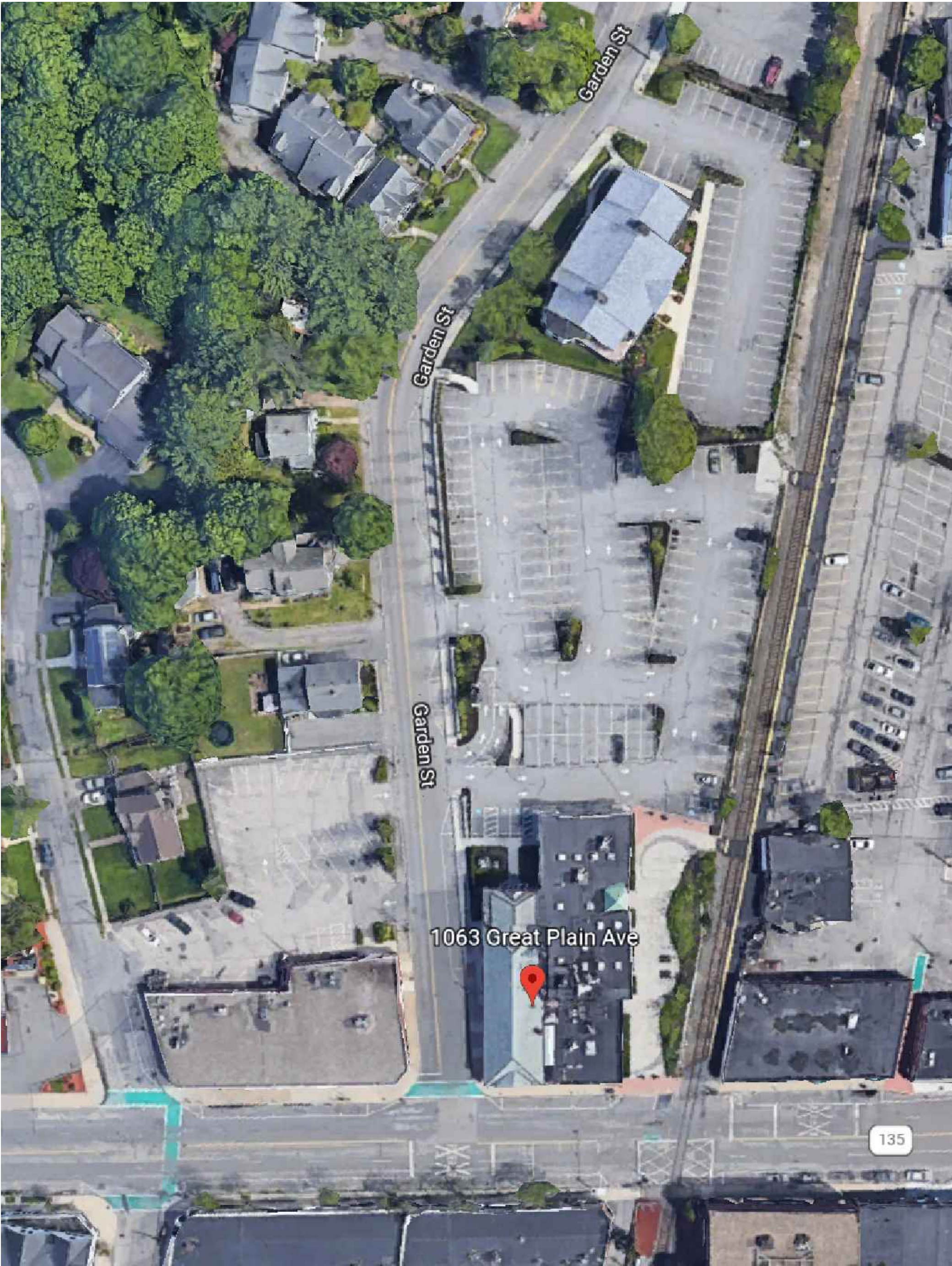
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2K22.003



A1.01

Main Level
Floor Plan



3 EXISTING SITE AREA



2 GARDEN ST. VIEW LOOKING EAST

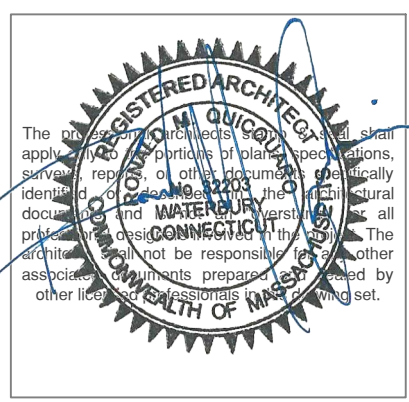


1 GARDEN ST. VIEW LOOKING WEST

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INTERIOR & EXTERIOR RENOVATIONS FOR:

NB Needham Bank
Build something.

1063 GREAT PLAIN AVE.

NEEDHAM, MA.

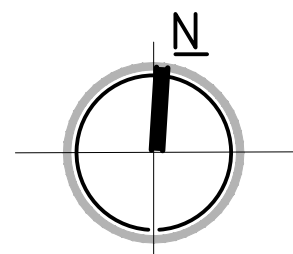
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Scale: 1/4" = 1'-0"

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A2.01
SITE AREA



NEW BRICK VENEER TO MATCH
EXISTING BUILDING ON SITE

NEW PRE-CAST UNIT MASONRY TO
MATCH EXISTING

NEW BRICK VENEER TO MATCH
EXISTING BUILDING ON SITE

NEW PRE-CAST UNIT MASONRY TO
MATCH EXISTING

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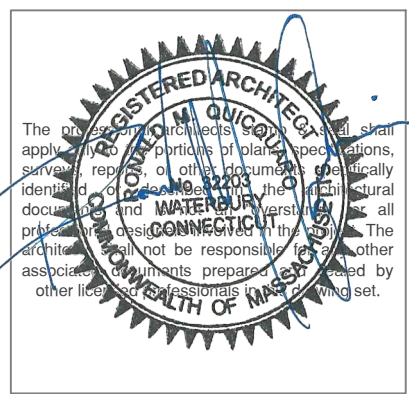
2 MATERIAL - MAJOR BUILDING ELEMENTS

1 MATERIAL - MAJOR BUILDING ELEMENTS

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Scale: 1/4" = 1'-0"

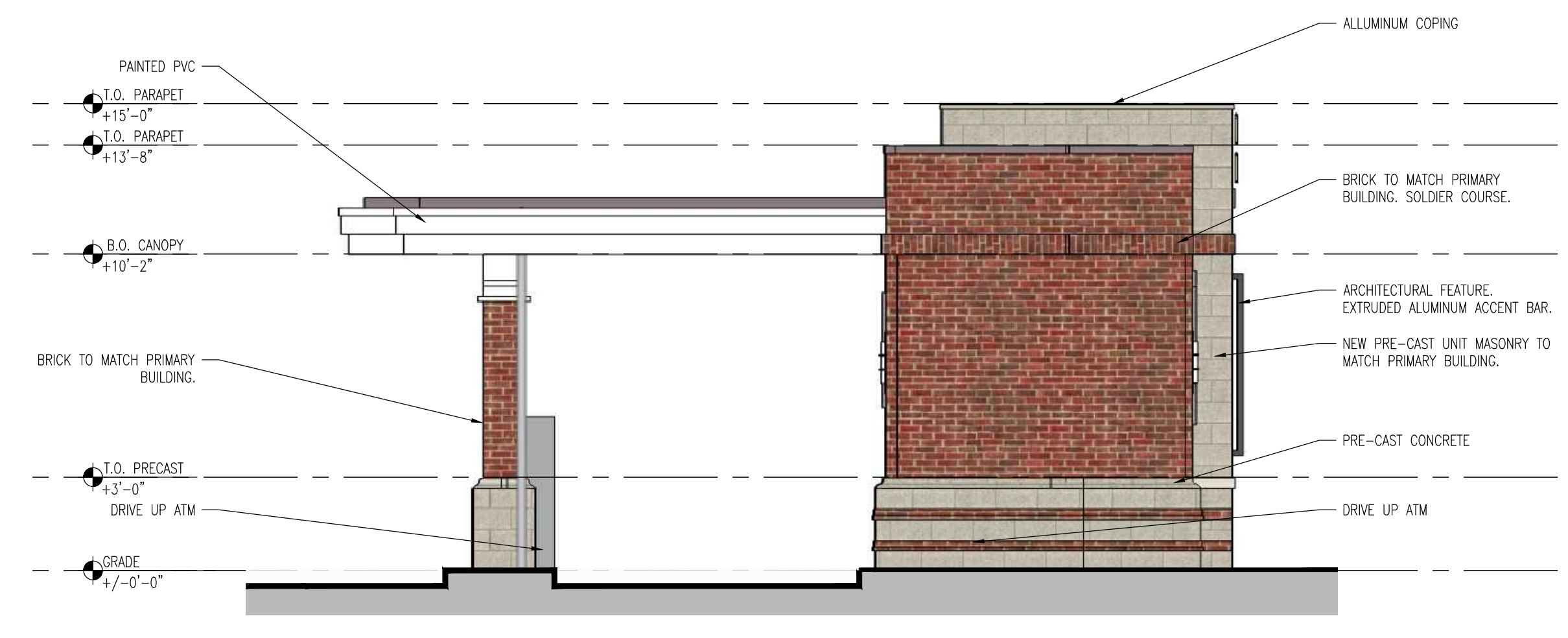
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2K22.003

A3.01
MATERIALS



4 WEST ELEVATION
SCALE: 1/4"=1'



3 NORTH ELEVATION
SCALE: 1/4"=1'



2 EAST ELEVATION
SCALE: 1/4"=1'



1 SOUTH ELEVATION
SCALE: 1/4"=1'



4 VIEW FROM GARDEN STREET
SCALE: N.T.S.



3 VIEW FROM GARDEN STREET
SCALE: N.T.S.



2 VIEW FROM PARKING LOT
SCALE: N.T.S.

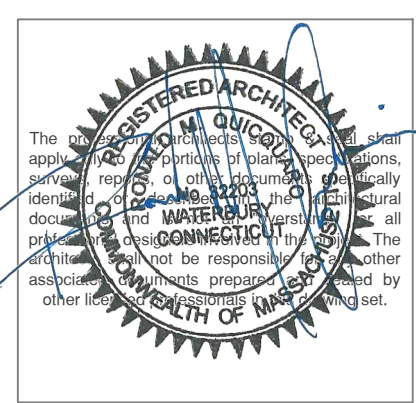


1 VIEW FROM EXISTING BUILDING REAR ENTRY
SCALE: N.T.S.

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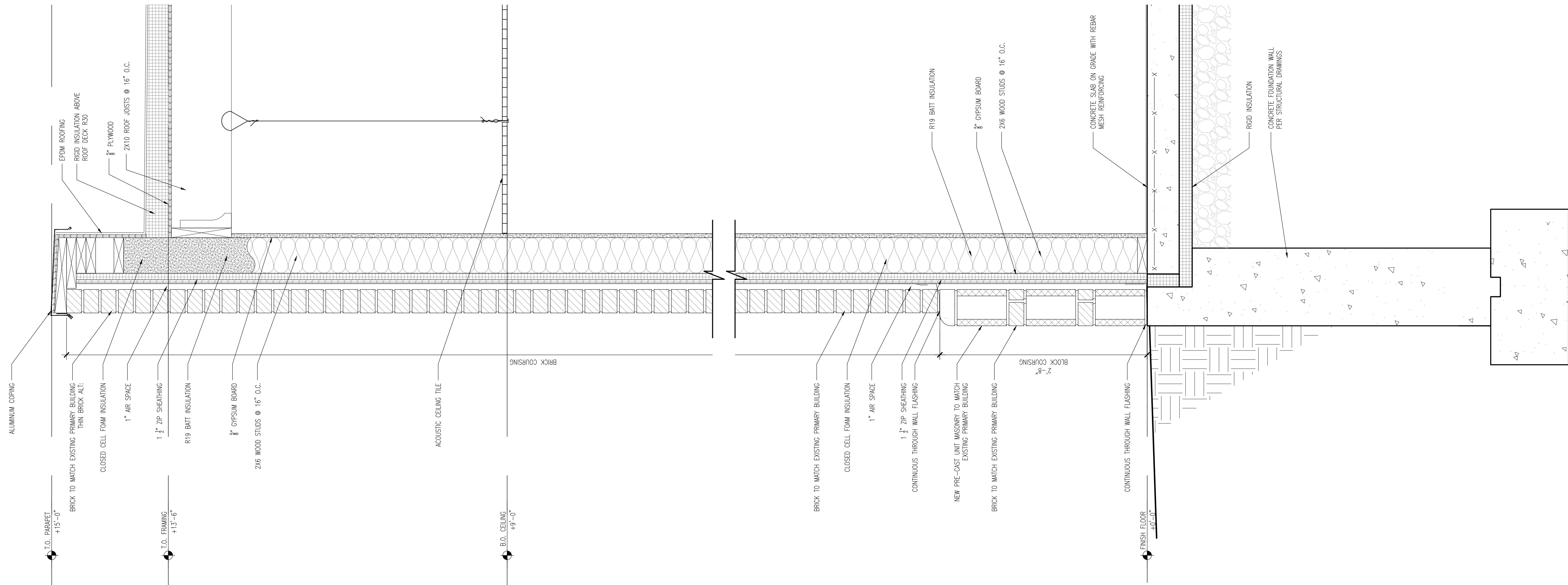
Date: JUNE 1, 2022

Scale: 1/4" = 1'-0"

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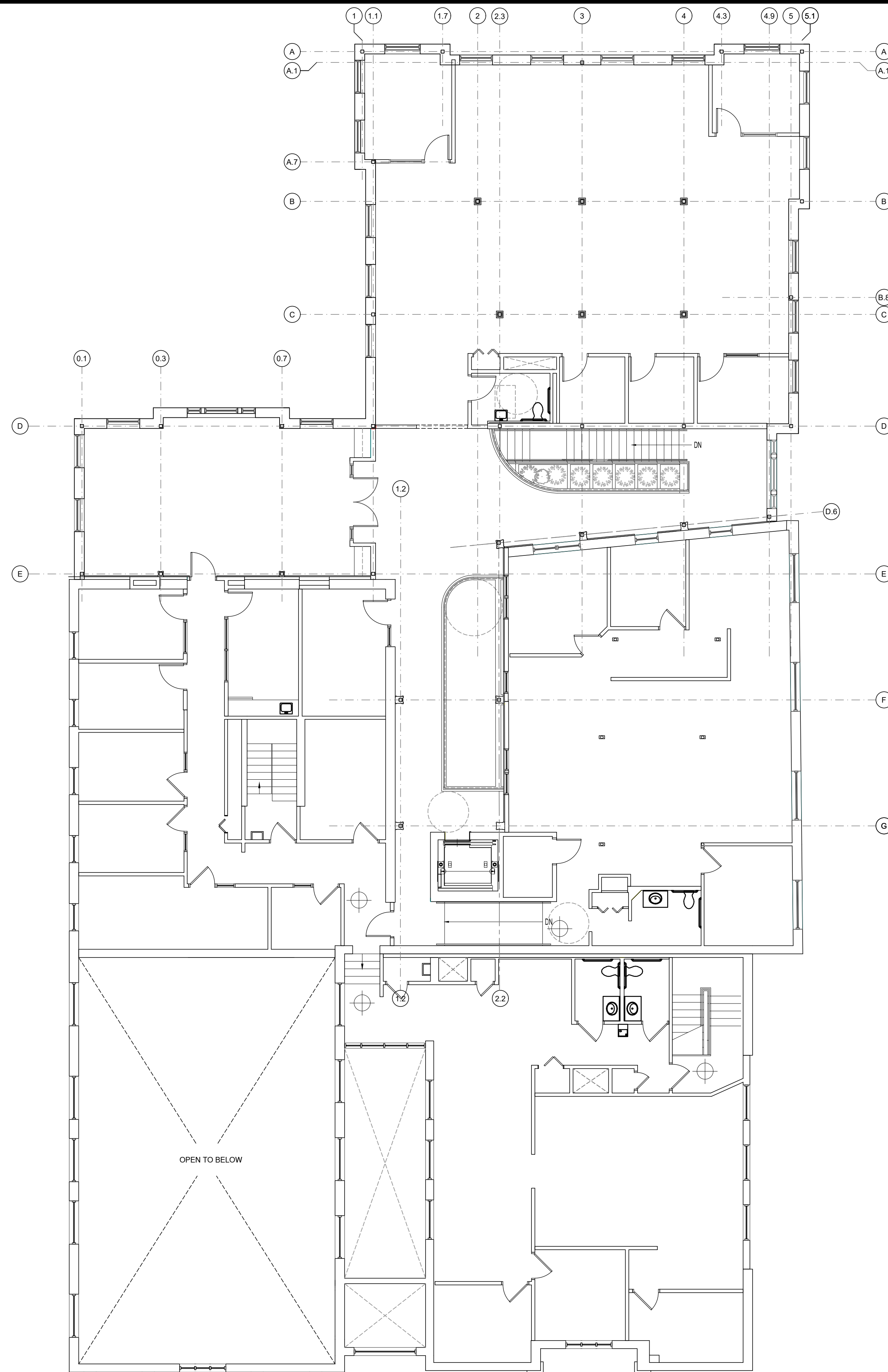
A5.01
3D VIEWS



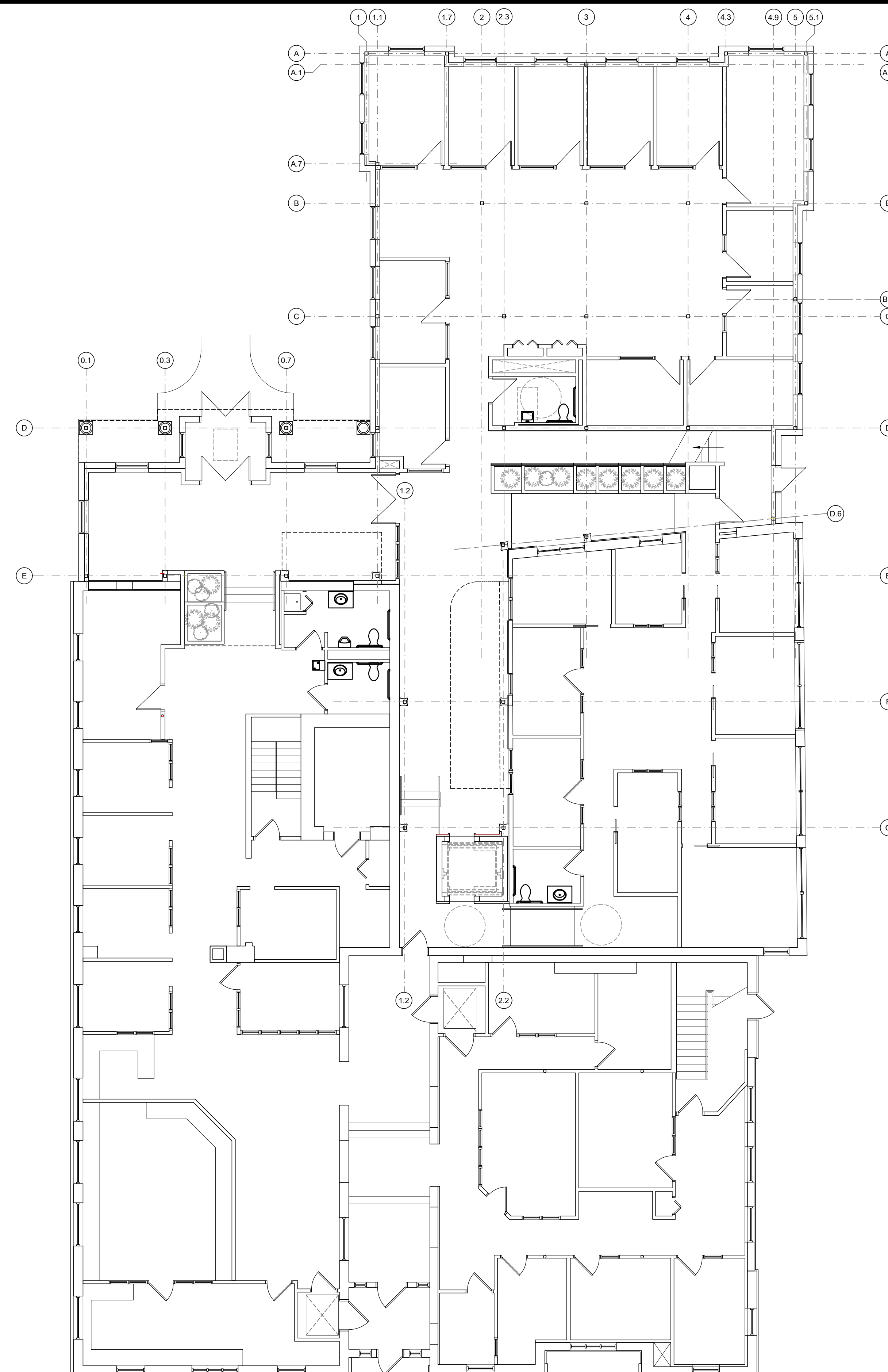
PRELIMINARY

THIS WALL SECTION IS SHOWN FOR PROCESS ONLY
AND IS NOT INTENDED FOR CONSTRUCTION

1 WALL SECTION
SCALE: 3/4" = 1'



2 SECOND FLOOR EXISTING PLAN
SCALE: NTS



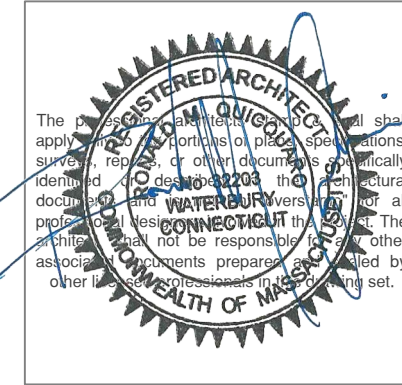
1 FIRST FLOOR EXISTING PLAN
SCALE: NTS

FOR REFERENCE ONLY

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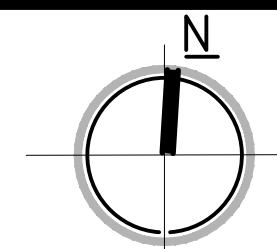
Issuances: 6.1.22 SITE PLAN SUBMISSION

Date: JUNE 1, 2022

Scale: 1/8" = 1'-0"

Drawn by:

Project No.
2K22.003



A1.00
Existing
Floor Plan

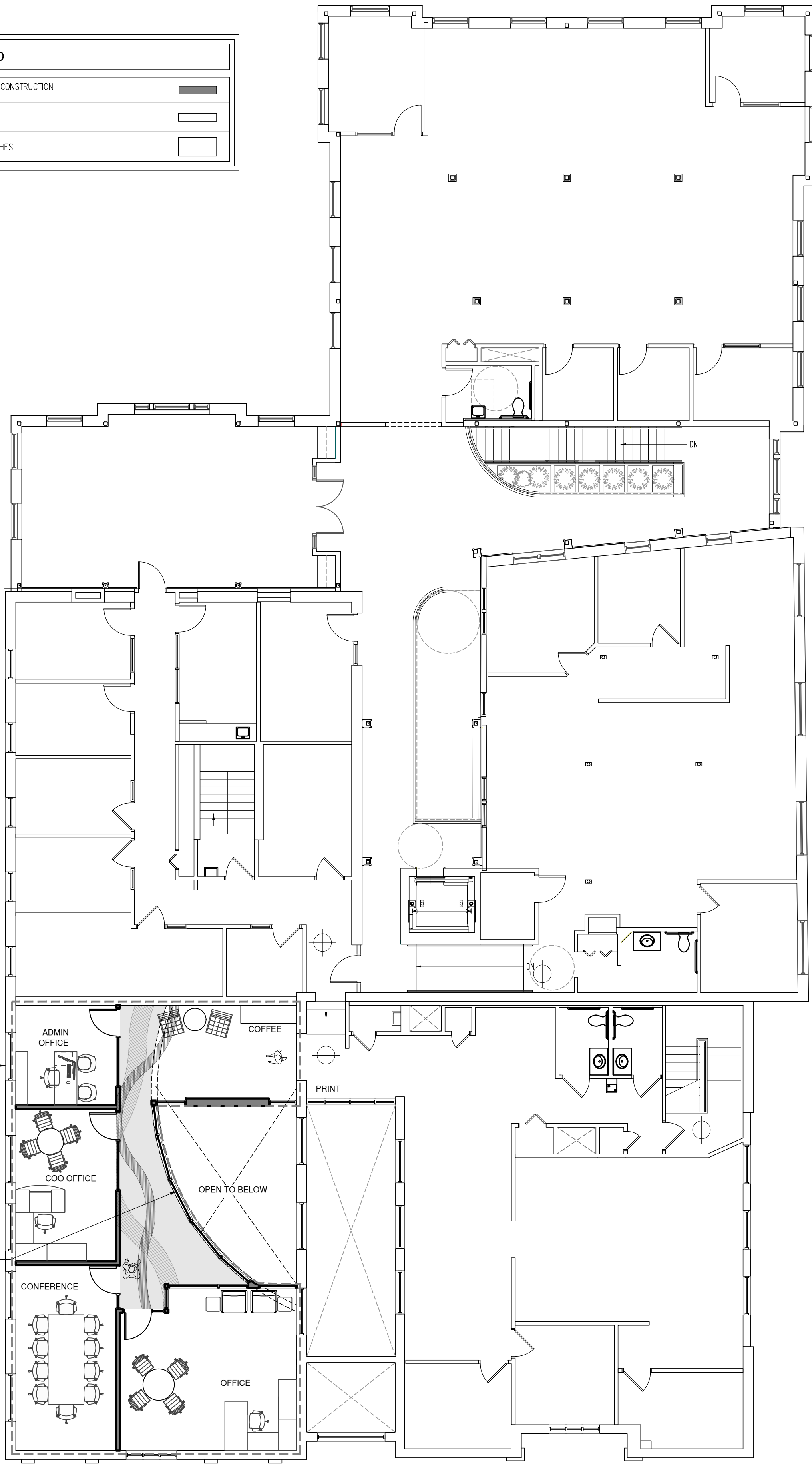
LEGEND

PROPOSED WALL CONSTRUCTION

EXISTING WALLS

NEW FLOOR FINISHES

NOTE:
NEW MEZZANINE
NET SQUARE
FOOTAGE 1,365 S.F.



2

SECOND FLOOR PLAN

SCALE: NTS

LEGEND

PROPOSED WALL CONSTRUCTION

EXISTING WALLS

NEW FLOOR FINISHES

LINE OF
GATE WHEN
CLOSED

ATM

RECEPTION

GATE

COFFEE

TECH KIOSK

BRANDING &
MARKETING
OPPORTUNITY

WORKROOM

CONFERENCE

OFFICE

CHECK

NEW TELLER

WAITING

OFFICE

CONFERENCE

OFFICE

OFFICE

OFFICE

OFFICE

ATM NIGHT
DEPOSIT

WORK AREA

NOTE:
EXISTING FIRST FLOOR
WITH PROPOSED
RENOVATIONS.
NO CHANGE TO
SQUARE FOOTAGE

1

FIRST FLOOR PLAN

SCALE: NTS

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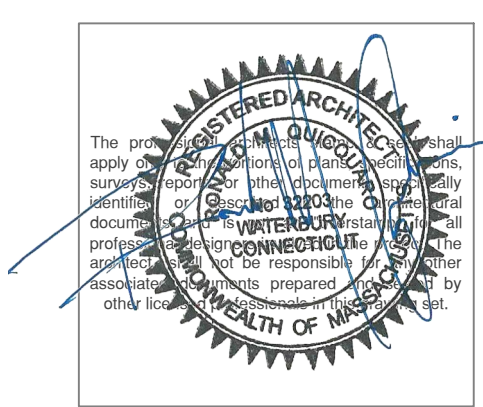
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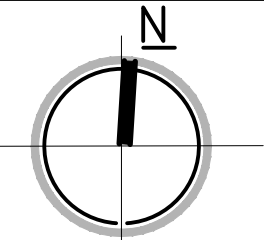
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A1.01

Main Level
Floor Plan